



# **WOKINGHAM BOROUGH COUNCIL**

A Meeting of the **PLANNING COMMITTEE** will be held at the Civic Offices, Shute End, Wokingham, RG40 1BN on **WEDNESDAY 4 MARCH 2015 AT 7.00 PM**

A handwritten signature in black ink, appearing to read 'Andy Couldrick', written in a cursive style.

Andy Couldrick  
Chief Executive  
Published on 24 February 2015

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## **Our Vision**

***A great place to live, an even better place to do business***

### ***Our Priorities***

**Improve educational attainment and focus on every child achieving their potential**

**Invest in regenerating towns and villages, support social and economic prosperity, whilst encouraging business growth**

**Ensure strong sustainable communities that are vibrant and supported by well designed development**

**Tackle traffic congestion in specific areas of the Borough**

**Improve the customer experience when accessing Council services**

### ***The Underpinning Principles***

**Offer excellent value for your Council Tax**

**Provide affordable homes**

**Look after the vulnerable**

**Improve health, wellbeing and quality of life**

**Maintain and improve the waste collection, recycling and fuel efficiency**

**Deliver quality in all that we do**

## MEMBERSHIP OF THE PLANNING COMMITTEE

### Councillors

Simon Weeks (Chairman)	Tim Holton (Vice-Chairman)	Chris Bowring
Lindsay Ferris	John Kaiser	Bob Pitts
Malcolm Richards	Rachelle Shepherd-DuBey	Chris Singleton

ITEM NO.	WARD	SUBJECT	PAGE NO.
102.		<b>APOLOGIES</b> To receive any apologies for absence.	
103.		<b>MINUTES OF PREVIOUS MEETING</b> To confirm the Minutes of the Meeting of the Committee held on 4 February 2015.	5 - 10
104.		<b>DECLARATION OF INTEREST</b> To receive any declaration of interest.	
105.		<b>APPLICATIONS TO BE DEFERRED AND WITHDRAWN ITEMS</b> To consider any recommendations to defer applications from the schedule and to note any applications that may have been withdrawn.	
106.	Wescott	<b>APPLICATION NO F/2014/2637 - LAND AT MARKET PLACE, PEACH STREET, AND ROSE STREET (REFERRED TO AS PEACH PLACE), WOKINGHAM</b> Recommendation: Conditional Approval, subject to Legal Agreement.	11 - 110
107.	Bulmershe and Whitegates	<b>APPLICATION NO: F/2014/2105 - FORMER ALLIED BAKERIES SITE, VISCOUNT WAY, WOODLEY</b> Recommendation: Conditional Approval, subject to Legal Agreement.	111 - 150
108.	Wokingham Without	<b>APPLICATION NO: F/2014/2865 - LAND AT HEATHLANDS, HEATHLANDS ROAD, WOKINGHAM</b> Recommendation: Refuse	151 - 172
109.		<b>PRE COMMITTEE SITE VISITS</b> To consider any recommendations from the Head of Development Management to hold pre-committee site visits, set out in Members' Update	
110.		<b>ANY OTHER ITEMS WHICH THE CHAIRMAN DECIDES ARE URGENT</b> A Supplementary Agenda will be issued by the Chief Executive if there are any other items to consider under this heading	

## **GLOSSARY OF TERMS**

The following abbreviations were used in the above Index and in reports.

<b>C/A</b>	Conditional Approval (grant planning permission)
<b>CAC</b>	Conservation Area Consent
<b>R</b>	Refuse (planning permission)
<b>LB</b>	(application for) Listed Building Consent
<b>S106</b>	Section 106 legal agreement between Council and applicant in accordance with the Town and Country Planning Act 1990
<b>F</b>	(application for) Full Planning Permission
<b>MU</b>	Members' Update circulated at the meeting
<b>RM</b>	Reserved Matters not approved when Outline Permission previously granted
<b>VAR</b>	Variation of a condition/conditions attached to a previous approval
<b>PS</b>	Performance Statistic Code for the Planning Application
<b>Category</b>	

### **CONTACT OFFICER**

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**MINUTES OF A MEETING OF THE  
PLANNING COMMITTEE  
HELD ON WEDNESDAY 4 FEBRUARY 2015 FROM 7.00PM TO 8:50PM**

*Present:- Simon Weeks (Chairman), Tim Holton, (Vice Chairman), Chris Bowring, Lindsay Ferris, John Kaiser, Bob Pitts, Malcolm Richards, and Rachelle Shepherd-DuBey*

*Also present:- Anthony Pollock*

**Officers:**

*Clare Lawrence, Head of Development and Regulatory Services*

*Chris Easton, Service Manager, Highway Development*

*Mary Severin, Borough Solicitor*

*Tricia Harcourt, Senior Democratic Services Officer*

*Case Officers: Graham Vaughan, Alex Thwaites, David Wetherill*

**MEMBERS UPDATE**

*There are a number of references to the Members' Update within these minutes. The Members' Update was circulated to all present prior to the meeting. It is available to view on the Council's website [www.wokingham.gov.uk](http://www.wokingham.gov.uk)*

**94. MINUTES**

The Minutes of the meeting of the Committee held on 10 December 2015 were confirmed as a correct record and signed by the Chairman.  
(the meeting on 7 January 2015 had been cancelled)

**95. APOLOGIES**

An apology for absence was submitted from Chris Singleton.

**96. DECLARATIONS OF INTEREST**

Tim Holton declared a pecuniary interest in Item 94 application F/2014/2013, extensions and changes of use at Henry Street Garden Centre, Arborfield, on the grounds that the Garden Centre is a client of the company he works for. He indicated that he would withdraw from the meeting for the discussion and decision making on this application.

**97. APPLICATIONS TO BE WITHDRAWN OR DEFERRED**

No applications had been recommended for deferral or withdrawn.

<b>98.</b>	<b>APPLICATION</b>	<b>O/2014/1386</b>
	Outline proposal for the erection of 16 dwellings with redevelopment of site and associated works following demolition of existing dwelling (All matters reserved)	
<b>at</b>	<b>134 -146 London Road, Ruscombe</b>	
<b>for</b>	<b>H W Hyde and Son</b>	

The Committee considered a report about this application set out on Agenda pages 4 to 28.

The Committee was advised that the Members' Update included details of:

- Recommended additional condition to limit hours of working on site;
- Amendment to condition 10;

- Amendment to Recommendations C and E;
- Clarification of the S106 contribution for affordable housing;
- Clarification around the provision of the footpath leading from the rear of the site;
- Clarification of statutory consultees' comments on drainage issues;
- Clarification of the suggested realignment of the boundary hedge.

The Committee was reminded that the application was purely an outline application which would establish the principle that 16 dwellings could be accommodated on this site. All details of access, layout, design, and landscaping will be considered through a future Reserved Matters application.

Jeremy Hardman, representing Ruscombe Parish Council, spoke objecting to the application.

Neil Davis, representing local residents, spoke objecting to the application.

Andrew Barr, agent spoke in support of the application.

It was noted that Members had visited the site on 30 January 2015 to assess the impact on the character of the area.

Many of the concerns expressed were in relation to matters which were not to be assessed as part of this application, such as drainage, screening, layout, potential overlooking, separation distances, and access. However, there was a general feeling among Members, the Parish Council and local residents that the suggested inclusion of a footpath from the south east corner of the site into Garraway Close was not necessary, as such a connection would be of no use to residents in the existing adjoining developments. Additionally, a footpath running along the boundary of Magnolia House would be detrimental to the amenity of the occupiers of that dwelling. It was suggested if this application was approved an informative be included to indicate that the footpath should not be included in any future Reserved Matters application.

**RESOLVED:** That application O/2014/1386 be approved, subject to:

- 1) the completion of a S106 Legal Agreement, as set out on Agenda pages 19 and 20, by 5 April 2015;
- 2) the completion of a reptile survey, and in the event that reptiles are found a scheme for the translocation or on-site migration be achieved;
- 3) the conditions set out on Agenda pages 4 to 9, and an additional condition to limit the hours of working on the site; and with condition 10 amended, both as set out in the Members' Update.
- 4) An additional informative to indicate that the footpath connection into Garraway Close, should not be included in any future Reserved Matters submission.

<b>99.</b>	<b>APPLICATION</b>	<b>F/2014/2323</b>
	Construction of nine detached dwellings and associated landscaping	
<b>at</b>	<b>Land north of Croft Road (rear of 89 – 95 Clares Green Road), Spencers Wood</b>	
<b>for</b>	<b>Darcliffe Homes Ltd</b>	

The Committee considered a report about this application set out on Agenda pages 29 to 60.

The Committee was advised that the applicant had made revisions to the proposals in light of comments from local residents; and that the Members' Update included details of:

- Heights of the dwellings on the nearest adjoining properties;
- The density of development in the surrounding area;
- Additional objection letters from neighbours;
- Confirmation of consultation responses from the Parish Council; Countryside Access/Public Rights of Way Officer; Urban Design & Conservation Officer/English Heritage; Housing; Natural England; and the Highways Agency;
- Recommended removal of condition 11, and amendments to condition 25;
- Clarification on the S106 contributions;

The following corrections to the report were noted;

- Agenda page 39, in Recommendation C – delete '*North Wokingham SDL*' and replace with '*South of M4 Wokingham SDL*';
- Agenda page 54, in the Conclusion – delete '*The reserved matters are..*' and replace with '*The proposed development is ..*'

Gordon Searl, representing himself and other local residents, spoke objecting to the application.

Matthew Jeal, representing the applicant, spoke in support of the application.

Anthony Pollock, a Local Ward Member, spoke objecting to the application.

It was noted that Members had visited the site on 30 January 2015 to assess the impact on the character of the area.

The Committee was reminded that this site was within the South of M4 SDL (Strategic Development Location), and was allocated for housing, but had not been included in the outline permission for the wider development site.

In response to concerns expressed locally, clarification was given that the density of development at 14 dwellings per hectare was considerably lower than the density expected in the wider SDL developments of between 30 and 35 dwellings per hectare. Also that revisions made in response to neighbours' concerns to reduce the height and bulk of some of the proposed dwellings, with dormer windows removed; and to move the dwelling on plot 1 away from the boundary. Now there are no three storey buildings; only plots 6, 7 and 9 are two and half storeys, the rest two storey, with plot 1 being considerably lower. The spacing of dwellings and rear garden lengths; comply with the design guidance; and parking provision exceeds the standards.

**RESOLVED:** That application F/2014/2323 be approved, subject to:

- 1) the completion of a S106 Legal Agreement, as set out in the Members' Update, by 31 March 2015;
- 2) the conditions set out on Agenda pages 31 to 42, with the condition 25 amended as set out in the Members' Update and condition 11 removed and subsequent conditions renumbered.

100.	APPLICATION	F/2014/2013
Extension to existing garden centre to provide relocated and extended restaurant, removal and replacement of garden centre building, additional open sided canopies, change of use		

of part of existing restaurant to retail area, change of use of part of existing retail area to restaurant use, larger outdoor sales area with fences and gates, changes to parking arrangements	
at	<b>Henry Street Garden Centre, Swallowfield Road, Arborfield</b>
for	<b>Mr T Goold</b>

Having declared a pecuniary interest in this item, Tim Holton left the meeting, before the Officer presentation and was not present during the discussion and decision making for this application.

The Committee considered a report about this application set out on Agenda pages 61 to 82.

The Committee was advised that the Members' Update include details of:

- Comparison of existing and additional floorspaces of the indoor retail, outdoor sales and restaurant areas;
- Effect of anticipated increased customer numbers;
- Peak occupancy of the car parking areas;
- Clarification of the existing retail restrictions which apply to the site.

Tim Goold, applicant spoke in support of the application.

In response to Members' questions, confirmation was given that the restriction of 49% maximum floor space for general retail use was to ensure that general retail activity is ancillary to the approved primary use as a garden centre. A garden centre is an appropriate use in a rural/countryside area, whereas unrestricted retail use is not.

**RESOLVED:** That application F/2014/1013 be approved, subject to the conditions set out on Agenda pages 62 to 65.

### **101. QUARTERLY ENFORCEMENT MONITORING REPORT**

The Committee received and noted the quarterly enforcement monitoring report set out on Agenda pages 83 and 84. Comment was made that the number of enforcement notices served was increasing as Officers are becoming more pro-active in cracking down on unauthorised development.

Members were informed that formal consultation on the proposed Local Planning Enforcement Plan would start on Monday 9 February 2015 for six weeks, with details available on the Council's website.

### **102. PRE-COMMITTEE SITE VISITS**

The Head of Development Management and Regulatory Services recommended that pre-Committee site visits be undertaken in respect of the following applications:

- F/2014/2637 – Land at Market Place, Peach Street and Rose Street (referred to as Peach Place), Wokingham for the redevelopment and refurbishment of existing buildings to provide mixed use development for town centre uses, as part of the regeneration of Wokingham Town Centre. A site visit and briefing is proposed on Wednesday 25 February 2015 to give Members the opportunity to gain a thorough understanding of this relatively complex proposal before it is reported to the planning Committee on 4 March 2015.

- F/2014/2105 – Former Allied Bakeries site, Viscount Way, Woodley – erection of 68 dwellings with associated roads, parking amenity space, landscaping and creation of new access onto Loddon Bridge Road. The site visit would enable Members to assess the impact on the character of the area, relationships with adjacent land uses and highways impact, and highway safety of the proposed new access.
- F/2014/2865 – land at Heathlands, Heathlands Road, Wokingham – erection of a two bedroomed single storey dwelling, including new roof on existing garage, following demolition of existing garage block. The site visit would enable Members to assess the impact on the character of the area and countryside and relationships with adjacent land uses.
- F/2014/2353 – land rear of 58 Hurst Road, Twyford – erection of 12 dwellings, including 4 affordable dwellings, with access, parking, open space and landscaping. The site visit would enable Members to assess the impact on the character of the area, neighbouring properties and potential drainage issues.
- F/2014/2784 – Green Isle, Wargrave Road, Remenham – erection of dwelling and boat house following the demolition of the existing dwelling and boathouse; plus alterations to existing footbridge. The site visit would enable Members to assess the impact on the character of the area.

**RESOLVED:** That pre-Committee site visits be undertaken on Wednesday 25 February 2015, and Friday 27 February 2015 in respect of the following applications:

- 1) F/2014/2637 – Land at Market Place, Peach Street and Rose Street (referred to as Peach Place), Wokingham for the redevelopment and refurbishment of existing buildings to provide mixed use development for town centre uses, as part of the regeneration of Wokingham Town Centre, visit on Wednesday 25 February 2015 to give Members the opportunity to gain a thorough understanding of this relatively complex proposal.
- 2) F/2014/2105 – Former Allied Bakeries site, Viscount Way, Woodley – erection of 68 dwellings with associated roads, parking amenity space, landscaping and creation of new access onto Loddon Bridge Road, to assess the impact on the character of the area, relationships with adjacent land uses and highways impact, and highway safety of the proposed new access.
- 3) F/2014/2865 – land at Heathlands, Heathlands Road, Wokingham – erection of a two bedroomed single storey dwelling, including new roof on existing garage, following demolition of existing garage block, to assess the impact on the character of the area and countryside and relationships with adjacent land uses.
- 4) F/2014/2353 – land rear of 58 Hurst Road, Twyford – erection of 12 dwellings, including 4 affordable dwellings, with access, parking, open space and landscaping, to assess the impact on the character of the area, neighbouring properties and potential drainage issues.
- 5) F/2014/2784 – Green Isle, Wargrave Road, Remenham – erection of dwelling and boat house following the demolition of the existing dwelling and boathouse; plus alterations to existing footbridge, to assess the impact on the character of the area.

*These are the Minutes of a meeting of the Planning Committee*

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# Agenda Item 106.

				<b>ITEM NO:</b>
<b>Development Management Ref No</b>	<b>No weeks on day of committee</b>	<b>Parish</b>	<b>Ward</b>	<b>Major Development Proposal:</b>
F/2014/2637	Planning performance agreement	Wokingham	Wescott	Council application for more than 100m <sup>2</sup>

**Applicant Location**      Wokingham Borough Council/WBD (joint applicants)  
 Land at Market Place Peach Street and Rose Street (referred to as Peach Place), Wokingham.      **Postcode**

**Proposal**      Part redevelopment and part retention and refurbishment of existing buildings to provide a mixed use development for town centre uses including Class A1 shops, Class A2 financial and professional services, Class A3 restaurants and cafés, Class A4 drinking establishments, Class A5 hot food takeaways and a new public square, plus 26 Class C3 dwellings (a net gain of 21), as part of the regeneration of Wokingham Town Centre.

The proposals include:

- Demolition of 34-35 Market Place, 2-22 (even) Peach Street, 1-6 The Arcade and the wall along the Rose Street frontage; and the removal of Rose Street car park;
- Retention of 24-38 (even) Peach Street (Marks & Spencer, the Haka and Redan Public House);
- Retention and refurbishment of 36 Market Place (including two first-floor apartments) with external alterations; and 1-5 (odd) Rose Street (including two second-floor apartments) with external alterations;
- Erection of new buildings for the uses listed above, new public toilets and associated works including reconfiguration of part of Marks & Spencer car park.

**Type**      Full  
**PS Category**      4 Major Developments, Retail, distribution and servicing  
**Officer**      Emy Circuit

**FOR CONSIDERATION BY**      Planning Committee on 4 March 2015  
**REPORT PREPARED BY**      Head of Strategic Development Location Delivery (Delivery Programme Director)

<b>SUMMARY</b>
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<p>The application site lies on the north side of the Market Place, extending along Peach Street and Rose Street up to and including 28-38 Peach Street (Marks &amp; Spencer). It forms part of a larger site identified by Development Plan documents for comprehensive regeneration and commonly referred to as “<i>Peach Place</i>”. The first phase of the regeneration of this part of the town centre has already taken place with the refurbishment of the buildings on the corner of Market Place and Rose Street (38-42 Market Place - Clarkes, John Wood Sports and Boots) and the current application represents the second phase.</p>
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A previous application was submitted in late 2013 but was withdrawn prior to determination and the current proposals are intended to address concerns raised at that time; in particular the Redan PH, Haka and the Marks and Spencer (M&S) store are proposed to be retained within the current scheme. Broadly, the proposals consist of refurbishment and extension of 1-5 Rose Street (Boots Opticians and Strange Jewellers), refurbishment of 36 Market Place (Superdrug and Costa Coffee), redevelopment of the terrace of buildings extending from 34 Market Place (Clinton Cards) around to 22 Peach Street (currently Mac Barbers), a new building between 5 Rose Street and the Methodist Church and formation of a new public square on the site of the Rose Street car park. 24-38 Peach Street (the Redan, Haka and M&S) are included in the application site boundary but the works proposed on this part of the site are limited to alterations to the configuration of M&S car park.

The proposals will result in a net increase of 1,221m<sup>2</sup> in the amount of floor space available for town centre uses, a net increase of 21 dwellings and a new 1,250m<sup>2</sup> public square; as a consequence a dental surgery, a small amount of office space and central, short-stay parking would be lost. The proposals are described in more detail in the appraisal section of this report.

The application is supported by a financial appraisal which demonstrates that the development would not be viable in normal circumstances. Although the package available for mitigation of infrastructure impacts is limited, this consideration is outweighed by the benefits to the town centre that the scheme will deliver (see paragraphs 151-160).

The application is before the Planning Committee because is a major development proposal and the Council is joint applicant.

## **PLANNING STATUS**

- Major Development Location of Wokingham (WBCS Policy CP9/MDDLDP Policy CC02)
- Major Town Centre (WBCS Policy CP13/ MDDLDP Policy TB15)
- Allocated "Peach Place" Mixed Use Site (MDDLDP Policy SAL08)
- Primary Shopping Area (MDDLDP Policy TB15)
- Primary shopping frontage (MDDLDP Policy TB15) (1-5 Rose Street, Market Place, Bush Walk, Peach Street and the Arcade)
- Wokingham Town Centre Conservation Area (MDDLDP Policy TB24)
- Archaeological Site (MDDLDP Policy TB24)
- 37 Market Place, the Old Bull and Bush (now Bush Walk) is grade II listed
- Green Route (MDDLDP Policy CC03) (Broad Street as far south as the junction of Rose Street)
- Thames Basin Heaths Special Protection Area 5/7km linear mitigation zone Core Strategy Policy CP08)
- Potentially contaminated Land/consultation zone (Dairy between Peach St and Rose St, Wokingham)
- Flood Zone 1
- Major Transport Schemes (WBCS Policy CP10/MDDLDP Policies CC08 & TB20)

## RECOMMENDATION

GRANT PLANNING PERMISSION subject to:

- A) the conditions below; and
- B) mitigation of infrastructure impacts as set out below, subject to
  - i. review three months prior to commencement
  - ii. review and payment of 90% of contributions, including the full contribution towards mitigation of the impact on the SPA which must be paid before occupation of any new dwelling, at practical completion; and
  - iii. final review and final payment two years after practical completion.

Infrastructure	Contribution
Access and Movement (My Journey sustainable travel)	£9,450
Education (primary and secondary)	£120,170
Mitigation of the impact upon SPA (ring-fenced contribution )	£35,590
Environmental Improvements in Wokingham Town Centre	c £192,790
<b>TOTAL</b>	<b>c £358,000</b>

### Conditions

#### Time for implementation

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

*Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).*

### Approved drawings

2. This permission is in respect of the following drawings:

Drawing No	Title	Received by the LPA
BNY-PD-08 GD LL01 A07	General Details	04/12/2014
BNY-PD-08 GP LL01 A06	Site Location Plan	12/02/2015
BNY-PD-08 GP LL02 A07	Application Boundary Plan	04/12/2014
<b>Existing Elevations</b>		
BNY-PD-05-LL01 A00	Existing Elevations - Demolition Drawing	
BNY-PD-08 GE LL01 A07	Existing Elevations	
<b>Existing Plans</b>		
BNY-PD-05-0002 A06	Existing Ground Floor Plan - Demolition	6/02/2015
BNY-PD-05-1002 A04	Existing First Floor Plan -	04/12/2014

	Demolition	
BNY-PD-08 GP 0002 A08	Existing Ground Floor Plan	06/02/2015
BNY-PD-08 GP 1002 A08	Existing First Floor Plan	06/02/2015
BNY-PD-08 GP 2002 A08	Existing Second Floor Plan	06/02/2015
BNY-PD-08 GP 3002 A05	Existing Roof Plan	04/12/2014
BNY-PD-08 GP LL03 A05	Existing Masterplan	04/12/2014
BNY-PD-08 GP LL04 A07	Existing Land Use Plan - Ground Floor	12/02/2014
BNY-PD-08 GP LL09 A08	Existing Land Use Plan - First Floor	12/02/2014
BNY-PD-08 GP LL13 A01	Existing Land Use Plan - Second Floor	06/02/2015
BNY-PD-08 GP LL14 A00	Existing Storey Heights	06/02/2015
<b>Proposed elevations</b>		
BNY-PD-08 GE LL04 A07	Proposed Detailed Elevations Sheet 1 - Rose Street	04/12/2014
BNY-PD-08 GE LL05 A07	Proposed Detailed Elevations Sheet 2 - Peach Street	04/12/2014
BNY-PD-08 GE LL06 A07	Proposed Detailed Elevations Sheet 3 - Peach Place Plaza	04/12/2014
BNY-PD-08 GE LL08 A09	Proposed Elevations	04/12/2014
BNY-PD-08 GE LL09 A10	Proposed Elevations	04/12/2014
BNY-PD-08 GE LL10 A07	Proposed Detailed Elevations Sheet 4 - Peach Place Arcade	04/12/2014
BNY-PD-08 GE LL11 A00	Peach Street Elevation - Existing and Proposed	12/02/2015
<b>Proposed Plans</b>		
BNY-PD-08 GP 0001 A29	Proposed Ground Floor Plan	12/02/2015
BNY-PD-08 GP 1001 A21	Proposed First Floor Plan	12/02/2015
BNY-PD-08 GP 2001 A18	Proposed Second Floor Plan	04/12/2014
BNY-PD-08 GP 3001 A10	Proposed Roof Plan	12/02/2015
BNY-PD-08 GP LL06 A09	Proposed Masterplan Ground Level - Site Location Plan	04/12/2014
BNY-PD-08 GP LL08 A08	Proposed Storey Heights	04/12/2014

<b>Sections</b>		
BNY-PD-08 GS LL01 A06	Existing Sections	04/12/2014
BNY-PD-08 GS LL02 A10	Proposed Sections	04/12/2014
<b>Levels</b>		
BNY-PD-08 GP LL05 A04	Existing Levels	04/12/2014
BNY-PD-08 GP LL07 A06	Proposed Levels	04/12/2014
<b>Parking and Servicing</b>		
BNY-PD-08 GP LL10 A08	Servicing	06/02/2015
BNY-PD-08 GP LL12 A00	Rose St Servicing Strategy	06/02/2015
BNY-PD-08 GP LL15 A00	Disabled Parking Provision Plan	06/02/2015
<b>Public Open Space</b>		
BNY-PD-08 GP LL16 A00	Public Open Space Area excluding Access	06/02/2015
BNY-PD-08 GP LL17 A00	Public Open Space Area including Access	06/02/2015

The development shall be carried out in accordance with the approved details unless otherwise agreed in writing by the local planning authority.

*Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.*

### Use of the commercial units

- Notwithstanding the provisions of Part 3 of the Second Schedule the Town and Country Planning (General Permitted Development) Order 1995 (as amended) (or any Order revoking and re-enacting that Order with or without modification), the ground-floor commercial units identified as units SU001-SU006 shall be used only as Class A1 shops and for no other purpose, including any other purpose in Class A of the Schedule to the Town and Country Planning [Use Classes] Order 1987 (as amended) or in any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order [with or without modification].

*In granting this permission the local planning authority has had regard to the special circumstances of this case, being the position in the primary shopping area and a primary shopping frontage, and wishes to have the opportunity of exercising control over any subsequent alternative use in the interests of the amenities of the area and vitality and retail character of the town centre.*

*Relevant policy: Core Strategy policies CP13 and CP14, Managing Development Delivery Local Plan policy TB15 and the Wokingham Town Centre Masterplan SPD.*

### Phasing

4. Should development (and therefore the submission of details to comply with conditions) be phased a phasing strategy shall first be submitted and approved in writing by the Local Planning Authority. The phasing strategy will define:
- i) the development to be delivered in each phase;
  - ii) the sequence of development; and
  - iii) how earlier phases of the development will be able to operate satisfactorily while later phases are still under construction.

*Reason: to ensure comprehensive planning of the site and discharge of conditions. Also to demonstrate that early phases can function satisfactorily while phases are delivered.*

*Relevant Policies: Core Strategy policies CP1, CP3, CP4 and CP14.*

### Levels

5. No development shall take place until a measured survey of the site and a plan at scale of not less than 1:500 showing details of existing and proposed finished ground levels within and surrounding the site (in relation to a fixed datum point) and finished roof levels has been submitted to and approved in writing by the local planning authority, and development shall be carried out in accordance with the approved details.

*Reason: In order to ensure a satisfactory form of development relative to surrounding buildings and landscape.*

*Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policies TB21 and TB24.*

### Demolition

6. The demolition hereby permitted shall not be undertaken before a contract for the carrying out of the redevelopment of the site is in place.

*Reason: to prevent premature demolition, in the interests of the character of the Wokingham Town Centre Conservation Area.*

*Relevant Policies: National Planning Policy Framework Section 12 (Conserving and Enhancing the Historic Environment), Core Strategy policies CP1 and CP3, Managing Development Delivery Local Plan policy TB 24 and the Wokingham Town Centre Masterplan SPD.*

### External Materials

7. Before any phase of the development hereby permitted is commenced, samples and details of the materials to be used in the construction of the external surfaces of the buildings in that phase shall be submitted to and approved in writing by the local planning authority. Development shall be carried out in accordance with the approved details.

*Reason: in the interests of visual amenity and to safeguard the character of the conservation area in which the property is located.*

*Relevant policy: National Planning Policy Framework Section 12 (Conserving and Enhancing the Historic Environment), Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy TB 24.*

8. Before any phase of the development hereby permitted is commenced, sample panel(s) of brickwork showing the proposed brick, method of bonding, colour of mortar and type of pointing to be used in that phase shall be prepared on site and approved in writing by the local planning authority. Development shall be carried out in accordance with the approved details.

*Reason: in the interests of visual amenity and to safeguard the character of the conservation area in which the property is located.*

*Relevant policy: National Planning Policy Framework Section 12 (Conserving and Enhancing the Historic Environment), Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy TB 24.*

#### Fenestration

9. For each phase of the development, detailed drawings and/or samples, as appropriate, in respect of all new fenestration, including full sized moulding profiles, shall be submitted to and approved in writing by the local planning authority before that phase of the development is begun and the work shall be carried in accordance with the approved drawings and samples.

*Reason: in the interests of visual amenity and to safeguard the character of the conservation area in which the property is located.*

*Relevant policy: National Planning Policy Framework Section 12 (Conserving and Enhancing the Historic Environment), Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy TB 24.*

10. Before any phase the development hereby permitted is commenced, a scheme identifying areas of clear and obscure glazing, together with details of the obscure glazing in that phase, shall be submitted to and approved in writing by the Local Planning Authority and work shall be carried out in accordance with the approved details.

*Reason: to achieve an appropriate balance between privacy and surveillance and to prevent storage of items, in particular in stockrooms, adjacent to windows detracting from the appearance of the development.*

*Relevant Policies: Core Strategy Policies CP1 and CP3.*

#### Detailed Design and visual amenity

11. Before any phase of the development hereby permitted is commenced, detailed drawings (at scale 1:100 or greater) showing all elevations of the development in that phase and including materials, detailing of the elevations and reveal depths shall be submitted to and approved in writing by the Local Planning Authority . Development shall be carried out in accordance with the approved details.

*Reason: in the interests of visual amenity and to safeguard the character of the*

*conservation area in which the property is located.*

*Relevant policy: National Planning Policy Framework Section 12 (Conserving and Enhancing the Historic Environment), Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy TB 24.*

12. Before the development hereby permitted is commenced, the design off SU010/APT shall be reviewed, with particular regard to maintaining views of the rear of the former stable range of the Bush Hotel, now forming Bush Walk, and of the town hall beyond. Alternative options shall be submitted to the Local Planning Authority for assessment and development shall be carried out in accordance with whichever revised scheme is approved.

*Reason: make sure opportunities are taken to retain visual links to the historic townscape which contribute towards a strong sense of place.*

*Relevant policy: National Planning Policy Framework Section 12 (Conserving and Enhancing the Historic Environment) and Managing Development Delivery Local Plan policy TB24*

13. Prior to the commencement of the relevant part of the work the following details shall be submitted to and approved in writing by the local planning authority and work shall be carried out in accordance with the approved details.
- a) Townhouse dormer roof construction (identified on the approved "Proposed Ground Floor Plan as 1-4 Rose Street);
  - b) Decorative brick treatment to triple gabled building (SU 006);
  - c) Rainwater and drainage goods;

*Reason: In the interests of visual amenity and to safeguard the character of the conservation area in which the property is located.*

*Relevant policy: National Planning Policy Framework Section 12 (Conserving and Enhancing the Historic Environment) and Managing Development Delivery Local Plan policy TB24*

14. Works to the façade and roof of 36 Market Place (Superdrug and Costa Coffee) shall not commence until areas of wall coverings, plaster, etc. that have first been approved in writing by the Local Planning Authority have been carefully stripped by hand to reveal any underlying historic (i.e. pre 19th century) timber frame that may exist. In the event of a frame being found, no part of the frame shall be cut or removed, and a revised Scheme incorporating the retention of the frame shall be submitted to the local planning authority for approval in writing. Works shall be carried out in accordance with the approved revised Scheme.

*Reason: In order to safeguard any special architectural or historic interest of the building.*

*Relevant policy: National Planning Policy Framework Section 12 (Conserving and Enhancing the Historic Environment) and Managing Development Delivery Local Plan policy TB24*

15. No historic (i.e. pre-19th century) roof timbers or joints between historic roof timbers of 36 Market Place (Superdrug and Costa Coffee) shall be sawn in connection with the works hereby approved without the prior express consent in writing of the local planning authority. Any dismantled joinery shall be carefully stored under weatherproof and secure covers on the site until required for reuse in connection with the works hereby approved.

*Reason: In order to safeguard any special architectural or historic interest of the building.*

*Relevant policy: National Planning Policy Framework Section 12 (Conserving and Enhancing the Historic Environment) and Managing Development Delivery Local Plan policy TB24.*

16. Notwithstanding the provisions the Town and Country Planning (General Permitted Development) Order 1995 (as amended) (or any Order revoking and re-enacting that Order with or without modification), no plant shall be installed outside the two zones identified on Drawing No BNY-PD-08-GP-3001 Rev A10, Proposed Roof Plan without the prior written consent of the Local Planning Authority.

*Reason: In the interests of visual amenity and to safeguard the character of the conservation area in which the development is located.*

*Relevant policy: National Planning Policy Framework Section 12 (Conserving and Enhancing the Historic Environment), Core Strategy policies CP1, CP3 and CP14, Managing Development Delivery Local Plan policy TB24 and the Wokingham Town Centre Masterplan SPD.*

#### Shop fronts and signage

17. All new shop fronts and signage shall be in accordance with a Shop Front and Signage Design Guide which shall be consistent with the advice in the Borough Design Guide SPD, June 2012 (or any guidance that supersedes it) and has first been submitted to and approved in writing by the Local Planning Authority. The Guide shall remain in force for the lifetime of the development.

*Reason: in the interests of visual amenity.*

*Relevant Policies: Core Strategy Policies CP1, CP3 and CP14; Managing Development Delivery Local Plan policy TB24 and the Wokingham Town Centre Masterplan SDP.*

#### Landscaping

18. i) Before the development of external spaces within the development (including the square, pedestrian access routes and reconfigured car park) hereby permitted are commenced, full details of both hard and soft landscape proposals shall be submitted to and approved in writing by the local planning authority. These details shall include:
- a) Scheme drawings;
  - b) Samples of hard landscaping materials;
  - c) Specification for tree pits and use of structural soils under paving;
  - d) Means of enclosure including the front and rear amenity spaces for the

four new houses and the service area at the rear of 1-5 Rose Street.

- e) Gates, bollards or other means of controlling access required to manage access to and servicing of the site as required by Condition 23;
  - f) Way-finding signage;
  - g) External lighting (other than illuminated signage which is controlled under advertisement regulations);
  - h) Power, water distribution points and drainage to facilitate external events;
  - i) Means of suspending canopies, banners and temporary lighting;
  - j) vehicle and pedestrian access and circulation areas
  - k) minor artefacts and structures including street furniture;
  - l) cycle parking to comply with Condition 27; and
  - m) phasing of implementation if relevant.
- ii) Before any phase of development including roof terraces and walkways or plant zones is commenced, details of the following shall be submitted to and improved in writing by the Local Planning Authority:
- n) surfacing; and
  - o) means of enclosure.
- iii) Soft landscaping details shall include planting plan, specification (including cultivation and other operations associated with plant and grass establishment), schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate, and implementation timetable.
- iv) All hard and soft landscape works shall be carried out in accordance with the approved details prior to the occupation of any part of the development or in accordance with a timetable approved in writing by the local planning authority.
- v) Any trees or plants which, within a period of five years after planting, are removed die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.
- vi) The means of controlling access to the site shall be erected in accordance with the approved details prior to occupation of any part of the development (other than existing commercial units which are retained and continue to trade during the redevelopment) and retained thereafter. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 1995 (as amended) (or any Order revoking and re-enacting that Order with or without modification), no other gates, barriers or other means of controlling access shall be erected without the prior written consent of the Local Planning Authority.

*Reason: in the interests of visual amenity and to ensure that the new civic space fulfils its role in the renaissance of Wokingham Town Centre.*

*Relevant policy: Core Strategy policies CP1, CP3, CP6 and CP14; Managing Development Delivery Local Plan policies CC03, TB21 and TB24 and the Wokingham Town Centre Masterplan SPD.*

19. No trees, shrubs or hedges within the site which are shown as being retained on the approved plans shall be felled, uprooted wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the local planning authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 5 years from the completion of the development hereby permitted shall be replaced with trees, shrubs or hedge plants of similar size and species unless the local planning authority gives written consent to any variation.

*Reason: to secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area.*

*Relevant policy: Core Strategy policy CP3 and CP14; Managing Development Delivery Local Plan policies CC03 and TB21 and the Wokingham Town Centre Masterplan SPD.*

20. a) No development or other operation shall commence on site until an Arboricultural Method Statement and Scheme of Works which provides for the retention and protection of trees, shrubs and hedges growing on or adjacent to the site in accordance with BS5837: 2012 has been submitted to and approved in writing by the local planning authority. No development or other operations shall take place except in complete accordance with the details as so-approved (hereinafter referred to as the Approved Scheme).
- b) No operations shall commence on site in connection with development hereby approved (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) until the tree protection works required by the Approved Scheme are in place on site.
- c) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.
- d) The fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval in writing of the local planning authority has first been sought and obtained.

*Reason: to secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence*

*Relevant policy: Core Strategy policy CP3 and CP14; Managing Development Delivery Local Plan policies CC03 and TB21 and the Wokingham Town Centre Masterplan SPD.*

21. Prior to the commencement of the development a landscape management plan, including long term design objectives, management responsibilities, timescales and maintenance schedules for all landscape areas, other than privately owned, domestic gardens, shall be submitted to and approved in writing by the local planning authority. The landscape management plan shall be carried out as approved.

*Reason: in order to ensure that provision is made to allow satisfactory maintenance of the landscaping hereby approved.*

*Relevant policy: Core Strategy policy CP3 and CP14; Managing Development Delivery Local Plan policies CC03 and TB21 and the Wokingham Town Centre Masterplan SPD.*

#### Access and servicing

22. No part of the development hereby permitted (other than existing commercial units which are retained and continue to trade during the redevelopment) shall be occupied until access has been constructed, the square is available for use by delivery and service vehicles and they are able to enter and leave the site in a forward gear, in accordance with the approved details. This provision shall be retained in accordance with the approved thereafter.

*Reason: to provide adequate off-street servicing plus turning to allow vehicles to enter and leave the site in a forward gear, to deter on-street servicing in the interests of road safety and convenience and the amenity of the town centre.*

*Relevant policy: Core Strategy policies CP3 & CP6, Managing Development Delivery Local Plan policy TB20 and the Wokingham Town Centre Masterplan SPD.*

23. No part of the development hereby permitted (other than existing commercial units which are retained and continue to trade during the redevelopment) shall be occupied until a Servicing Management Plan has been submitted to and approved in writing by the Local Planning Authority. The Servicing Management Plan shall include details of both physical and management measures for controlling deliveries in order to avoid disturbance to residents within and near to the development and conflict between delivery/service vehicles and pedestrians using the square or pedestrian routes through the site. In any case no deliveries shall be taken in or dispatched from the site outside the hours of 07:00 and 21:00 Monday to Saturdays nor at any time on Sundays, Bank or Public Holidays.

*Reason: in the interests of residential amenity, pedestrian safety, highway safety and convenience and the character and amenity of the town centre generally.*

*Relevant Policies: Core Strategy Policies CP1, CP3, CP6, CP14; Managing Development Delivery Local Plan Policies CC06 and TB20; and the Wokingham Town Centre Masterplan SPD.*

24. No building (other than existing commercial units which are retained and continue to trade during the redevelopment) shall be occupied until refuse and recycling storage facilities have been provided in accordance with the approved details. These facilities shall be permanently retained and used for no purpose other than

the temporary storage of refuse and recyclable materials.

*Reason: to ensure the efficient collection of waste materials whilst avoiding highway obstruction and loss of visual amenity, in the interests of a functional development, the character of the area, highway safety & convenience and the quality of the pedestrian environment.*

*Relevant policy: Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC04.*

25. Before the development hereby permitted is commenced, details of visibility splays of:
- a) at least 2.4 metres by 2.4 metres for pedestrians at the three egresses onto Rose Street and Peach Street; and
  - b) of at least 2.4 metres by 43 metres for vehicles at the servicing egress from the square onto Rose Street;
  - c) of at least 2.4 metres by 43 metres for vehicles at the servicing access/egress from the square; and
  - d) at least 2.4 metres by 15 metres for vehicles leaving the serving area at the rear of 1-5 Rose Street

shall be submitted to and approved in writing by the local planning authority unless it can be demonstrated that reduced visibility splays are appropriate based on speed limits and proposed speed control measures, in which case reduced visibility splays may be approved. Access shall be provided and the visibility splays cleared of any obstruction exceeding 0.6 metres in height in accordance with the approved details before occupation of the development (except for existing commercial units which are retained and continue to trade during the redevelopment). Access shall be retained in accordance with the approved details and used for no other purpose and the land within the visibility splays shall be maintained clear of any visual obstruction exceeding 0.6 metres in height at all times.

*Reason: in the interests of highway safety and convenience.*

*Relevant policy: Core Strategy policies CP3 & CP6.*

#### Disabled parking provision

26. Before the use of the Rose Street Car Park by visiting members of the public ceases, three disabled parking bays shall be provided (to replace those displaced by the development) in a suitable alternative location within the town centre, in accordance with details that have first been submitted and approved in writing by the Local Planning Authority.

*Reason: to ensure that the development does not disadvantage people with mobility impairment.*

*Relevant Policies: Core Strategy Policies CP1, CP2 and CP6.*

### Cycle storage

27. Before the development hereby permitted is commenced, details of:

- e) secure, covered and suitable located cycle storage for at least seven bicycles for people employed within the development;
- f) secure, covered and suitable located cycle storage for at least two bicycles for each dwelling (at least 52 in total); and
- g) secure cycle storage for at least 30 bicycles for visiting members of the public shall be submitted to and approved in writing by the local planning authority. Cycle storage shall be provided in accordance the approved details before occupation of the development it is to serve (for the avoidance of doubt this excludes existing commercial units which are retained and continue to trade during the redevelopment), and shall be permanently retained in the approved form for the parking of bicycles and used for no other purpose.

*Reason: to ensure provision of secure sheltered cycle storage, to support the use of sustainable travel.*

*Relevant policy: NPPF Section 4 (Sustainable Transport) and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.*

### Construction Environmental Management Plan

28. No development, including any works of demolition, shall take place until the appointed main contractor has submitted a detailed Construction Environmental Management Plan (CEMP) based on the submitted Outline CEMP Report dated December 2014 and the detailed CEMP has been approved in writing by Local Planning Authority. In addition to the issues identified in the Outline CEMP the document shall include:

- i) parking provision for site operatives and visitors;
- ii) provision for loading and unloading of plant and materials;
- iii) storage of plant and materials used in constructing the development;
- iv) the erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate;
- v) measures to prevent deposit of mud on the highway;
- vi) provision for servicing of commercial units which are to be retained and continue to trade during the redevelopment;
- vii) measures to minimise dust during demolition; and
- viii) access to the Bradbury Centre.

The approved CEMP shall be adhered to throughout the construction period.

*Reason: in the interests of highway safety and convenience and to minimise the impact on the amenity of local residents arising from dust, emissions and noise during the construction phase.*

*Relevant policy: Core Strategy policies CP3 & CP6.*

### Hours of construction

29. No work relating to the construction of the approved development, including works of demolition or preparation prior to building operations, shall take place other than:

- i) between the hours of 07:00 to 20:00 Monday to Saturday plus limited works (delivery and removal of materials and erection and dismantling of scaffolding) from 20:00 to 22:00;
- ii) limited works (delivery and removal of materials and erection and dismantling of scaffolding) between 08:00 and 13:00 on Sundays; and;
- iii) individual operations which cannot reasonably be undertaken within the construction working hours defined at i) and ii) and have been notified to the Local Planning Authority (including details of the nature extent and timetable for the works) at least two weeks in advance and agreed in writing (by exchange of letter).

Such operations may include:

- a) loading, unloading or deliveries;
  - b) erection or dismantling of scaffolding around shop fronts or on pavement areas;
  - c) demolition, soft strip and asbestos removal works;
  - d) new build activities; and
  - e) other construction works which reasonably need to be undertaken outside of hours defined at i) having regard to the circumstances of the site where these works would otherwise cause disruption to trade, pedestrian circulation or the free flow of traffic.
- iv) Where works are agreed by the LPA under iii), Wokingham Town Council and the residential properties within an identified zone that has first been submitted to and approved in writing by the Local Planning Authority shall be given written notice at least one week in advance of the works taking place. The notification shall include details of the nature, extent and timetable for the works and telephone number that the party responsible for the works can be contacted on for the duration of the works.

*Reason: to protect the occupants of neighbouring properties from noise and disturbance outside the permitted hours during the construction period.*

*Relevant Policies: Wokingham Borough Core Strategy Policies CP1 and CP3 and Managing Development Delivery Local Plan Policy CC06.*

### Hours of operation, noise and air quality

30. No customer shall be permitted on the Class A premises hereby permitted (other than the existing commercial units that are to be retained at 1-5 Rose Street, 36 Market Place and 24-38 Peach Street) outside the hours of 07:00 and 23:00. No customer shall be permitted to use outside areas ancillary to the use of the premises and doors and windows shall be kept shut outside the hours of 07:00 and 22:00.

*Reason: To safeguard residential amenities.*

*Relevant policy: Core Strategy policies CP1, CP3 and CP14 and Managing*

*Development Delivery Local Plan policy CC06.*

31. Before any phase of the development hereby permitted is commenced a noise attenuation scheme shall be submitted for the Class A premises within that phase and the approved measures shall be implemented in accordance with the approved details before first occupation of any of the Class A premises within that phase and be retained thereafter.

*Reason: To safeguard residential amenities.*

*Relevant policy: Core Strategy policies CP1, CP3 and CP14 and Managing Development Delivery Local Plan policy CC06.*

32. Before any phase of the development hereby permitted is commenced, schemes for protecting the proposed dwellings within that phase from
- i) road traffic noise;
  - ii) noise arising from the use of the new square (dwellings facing onto the square should be provided with acoustic ventilation as an alternative to opening windows); and
  - iii) poor air quality

shall be submitted to and approved in writing by the local planning authority. Works shall be carried out in the accordance with the approved schemes before first occupation of any dwelling that relies upon them.

*Reason: to ensure satisfactory noise attenuation measures and ventilation are installed.*

*Relevant policy: NPPF Section 11 (Conserving and Enhancing the Natural Environment), Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy CC06.*

33. Noise emitted from externally mounted plant and equipment shall not at any time exceed a level 5dB below the existing background level (LA90) (10dB if there is a tonal element) when measured at 1m from the facade of the nearest noise sensitive location.

*Reason: To safeguard residential amenities.*

*Relevant policy: Core Strategy policies CP1, CP3 and CP14 and Managing Development Delivery Local Plan policy CC06.*

#### Ventilation and Odour Control

34. Before any phase of the development hereby permitted is commenced, a ventilation strategy for that phase, to include fume extraction, mechanical ventilation and filtration equipment, shall be submitted to and approved in writing by the Local Planning Authority. The strategy shall include:
- i) details of the controls to be put in place to minimise odour from Class A3 cafes and restaurants, A4 drinking establishments and A5 Hot food takeaways) within the phase;

- ii) details of any external air conditioning/heating equipment;
- iii) specification of proposed equipment;
- iv) routing of ducting which should as far as possible be within the building envelope;
- v) provision for extraction equipment to be installed at a later date in units that are first occupied for A1 shop or A2 financial and professional services use.

The approved strategy shall be implemented in full before first occupation of any development within that phase (other than existing commercial units which are retained and continue to trade during the redevelopment) and the equipment shall thereafter be retained, operated and maintained in its approved form and in accordance with the manufacturer's recommendations for so long as the use hereby permitted remains on site.

*Reason: to minimise the impact on the amenity of local residents (new and existing) arising from odour from commercial kitchens and other commercial operations. Also to ensure that the installation of the equipment does not detract from the quality and appearance of the development.*

*Relevant Policies: Core Strategy Policies CP1, CP3 and CP14; Managing Development Delivery Local Plan policies CC06 and TB24 and the Wokingham Town Centre Masterplan SDP.*

#### Contamination

35. No development shall take place until a scheme to deal with contamination of the site (including phasing if relevant) has been submitted to and approved in writing by the local planning authority. The scheme shall include an investigation and assessment to identify the extent of contamination and the measures to be taken to avoid risk when the site is developed. Development shall not commence until the measures approved in the scheme have been implemented.

*Reason: to ensure that any contamination of the site is identified at the outset to allow remediation to protect existing/proposed occupants of property on the site and/or adjacent land.*

*Relevant policy: NPPF Section 11 (Conserving and Enhancing the Natural Environment) and Core Strategy policies CP1 & CP3.*

#### Archaeology

36. No development, other than demolition to ground level, shall take place until:
- i) the exploratory archaeological work as set out in the 'Written Scheme of Investigation for Archaeological Trial Trenching' prepared by URS and dated November 2014 (or a comparable scheme that has first been submitted to and approved in writing by the local planning authority) has been implemented; and
  - ii) a programme of archaeological mitigation resulting from the exploratory archaeological work has been submitted to and agreed in writing by the local planning authority. The programme shall be implemented in accordance with the approved details; and

- iii) a programme for post-excavation assessment, analysis, reporting, publication and archiving has been submitted to and approved in writing by the Local Planning Authority. The programme shall be implemented in accordance with the approved details.

*Reason: the site is identified as being of archaeological potential. Investigation is required to allow preservation and recording of any archaeological features or artefacts before disturbance by the development.*

*Relevant policy: National Planning Policy Framework Section 12 (Conserving and Enhancing the Historic Environment) and Managing Development Delivery Local Plan policy TB25*

### Ecology

37. Work shall be in accordance with paragraph 6.2 of the Peach Place Bat Survey Report (URS, December 2014) unless otherwise approved in writing by the local planning authority.

*Reason: to ensure appropriate protection of bats, which are a protected species, during development.*

*Relevant Policies: Wokingham Borough Core Strategy Policy CP7 and MDDL Policy TB23.*

38. Before the development hereby permitted is commenced, details of a swift (*Apus apus*) nesting box scheme (including at least 20 new Schwegler swift nest boxes or equivalent in suitable locations on the building) shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall be implemented in accordance with the approved details before occupation of any new or refurbished building and retained thereafter.

*Reason: to enhance biodiversity*

*Relevant Policies: Wokingham Borough Core Strategy Policy CP7 and MDDL Policy TB23.*

### Drainage

39. No development shall take place until full details of the Drainage System(s) have been submitted to and approved in writing by the Local Planning Authority. These shall include:
- a) full details of all phasing and/or measures proposed to mitigate risks of flooding and/or pollution incidents arising to receiving watercourses/bodies or neighbouring development throughout construction;
  - b) demonstration of where and how surface water attenuation and infiltration shall be provided across the site (to include tree pits, rain gardens and permeable paving) and that the attenuation features are adequately sized to serve the development (i.e. will not flood any of the proposed dwellings or neighbouring development) for all events up to the 1 in 100 year storm plus allowances for the effects of climate change, taking account of achievable discharge rates over the lifetime of the development;
  - c) demonstration that the design of the drainage system accounts for the likely

impacts of: climate change and changes in impermeable area, over the design life of the development;

- d) demonstration that the proposed development will not exacerbate the risk of surface water flooding off-site for all surface water flood events up to and including the 1 in 100 year event;
- e) full details of all components of the proposed drainage system including source control, conveyance, storage, flow control and discharge. Details shall include dimension, locations, reference to storm simulation files, gradients, invert and cover levels and drawings as appropriate. This shall be identified for all catchments;
- f) full details of water quality treatment components of the proposed drainage strategy. Details of component(s) including type, dimension, locations, capacity, maintenance requirements and frequency, gradients, invert and cover levels and drawings as appropriate. This shall be identified for all catchments;
- g) full details of the maintenance and/or adoption proposals /agreements for the development covering every aspect of the proposed drainage system including a schedule of inspections and issue of an annual inspection report.

*Reason: to prevent increased flood risk from surface water run-off.*

*Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10*

#### Sustainability

40. All of the dwellings within the development hereby approved shall meet internal potable water consumption targets of 105 litres or less per person per day. The seven new-build flats APT006-009, 2a, 2b and 3 shall achieve at least Code for Sustainable Homes Level 3; the 15 new build dwellings, flats 001-005, 010-015 and the four houses shall achieve at least Code for Sustainable Homes Level 4; the five refurbished flats 1, 2 and two "COSTA APT" shall achieve at least BREEAM Domestic Refurbishment Scheme "Very Good"; and the new-build Class A units SU001-SU016 shall achieve BREEAM retail "Very Good" (or such national measures of sustainability for house design that replace these schemes). None of these buildings shall be occupied until a Final Certificate has been issued for it by an accredited assessor certifying that the relevant score has been achieved.

*Reason: To ensure developments contribute to sustainable development.*

*Relevant Policies: Wokingham Borough Core Strategy Policy CP1, Managing Development Delivery Local Plan Policies CC04 and CC05, and the Sustainable Design and Construction Supplementary Planning Document.*

41. Prior to the commencement of development a scheme for generating at least 10% of the predicted energy requirement of the development from decentralised renewable and/or low carbon sources (as defined in the glossary of Planning Policy Statement: Planning and Climate Change (December 2007) or any subsequent version) shall be submitted to and approved in writing by the local planning authority. The approved scheme shall be implemented before the

development is first occupied and shall remain operational for the lifetime of the development.

*Reason: to ensure developments contribute to sustainable development.*

*Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1, Managing Development Delivery Local Plan policy CC05 & the Sustainable Design and Construction Supplementary Planning Document.*

#### Emergency Water supply

42. Before first occupation of any part of the development fire hydrants, or other suitable emergency water supplies, shall be provided in accordance with a scheme including details of their location, specification and a programme for their provision which has first been submitted to and approved in writing by the Local Planning Authority.

*Reason: To ensure that an adequate infrastructure is provided.*

*Relevant Policies: Core Strategy Policy CP4.*

#### Employment Skills Plan

43. Before the development hereby approved begins an employment skill plan shall be submitted to and approved in writing by the Local Planning Authority and the plan shall be implemented in accordance with the approved details.

*Relevant Policies: Managing Development Delivery Local Plan policy TB12.*

#### **Informatives**

1. The development accords with the policies contained within the adopted / development plan and there are no material considerations that warrant a different decision being taken.
2. You are advised, in compliance with The Town and Country Planning [Development Management Procedure] [England] Order 2010 that the following policies and/or proposals in the development plan are relevant to this decision:

#### **Wokingham Borough Core Strategy Policies:**

- CP1: Sustainable development
- CP2: Inclusive communities
- CP3: General Principles for development
- CP4: Infrastructure requirements
- CP5: Housing mix, density and affordability
- CP6: Managing travel demand
- CP7: Biodiversity
- CP8: Thames Basin Heaths Special Protection Area
- CP9: Scale and location of development proposals
- CP10: Improvements in the Strategic Transport Network
- CP13: Town centres and shopping
- CP14: Growth and renaissance of Wokingham Town Centre

CP17: Housing delivery

**Managing Development Delivery Local Plan Policies**

CC01: Presumption in Favour of Sustainable Development

CC02: Development Limits

CC03: Green Infrastructure, Trees and Landscaping

CC04: Sustainable Design and Construction

CC05: Renewable energy and decentralised energy networks

CC06: Noise

CC07: Parking

CC08: Safeguarding alignments of the Strategic Transport Network & Road Infrastructure

CC09: Development and Flood Risk (from all sources)

CC10: Sustainable Drainage

TB05: Housing Mix

TB07: Internal Space Standards

TB08: Open Space, sport and recreational facilities standards for residential development

TB09: Residential accommodation for vulnerable groups

TB12: Employment Skills Plan

TB15: Major Town and Small Town/District Centre development

TB16: Development for Town Centre Uses

TB19: Outdoor advertising

TB20: Service Arrangements and deliveries for Employment and Retail use

TB21: Landscape Character

TB23: Biodiversity and Development

TB24: Designated Heritage Assets (Listed Buildings, Historic Parks and Gardens, Scheduled Ancient Monuments and Conservation Areas)

SAL05: Delivery of avoidance measures for Thames Basin Heaths Special Protection Area

SAL08: Allocated Mixed Use Sites

**Supplementary Planning Documents and other guidance**

Wokingham Borough Council Borough Design Guide Supplementary Planning Document (June 2012)

Wokingham Town Centre Masterplan Supplementary Planning Document (April 2010)

Sustainable Design and Construction Supplementary Planning Document (May 2012)

Affordable Housing Supplementary Planning Document (July 2013)

Planning Advice Note, Infrastructure Impact Mitigation, contributions for New Development

Wokingham Town Centre and Langborough Road Conservation Areas Study

National Planning Policy Framework and Planning Practice Guidance

3. The applicant is reminded that a Demolition Notice may be required to be served on the Council in accordance with current Building Regulations and it is recommended that the Building Control Section be contacted for further advice.
4. This permission does not convey or imply any approval or consent that may be

required for the display of advertisements on the site for which a separate Advertisement Consent application may be required. You should be aware that the display of advertisements without the necessary consent is a criminal offence liable to criminal prosecution proceedings through the courts.

5. The Head of Technical Services at the Council Offices, Shute End, Wokingham [0118 9746000] should be contacted for the approval of the access construction details before any work is carried out within the highway (including verges and footways). This planning permission does NOT authorise the construction of such an access or works.
6. Before development is commenced a licence MUST be obtained under s.177/ s.178 of the Highways Act 1980 with respect to any part of the development which overhangs the highway. A licence must be obtained from the Council's Legal Department at Shute End, Wokingham.
7. Given the conservation area setting and aspiration to a high quality development, the roofing materials to comply with Condition 7 will be expected to be natural materials; natural slate and clay tiles.
8. The details to comply with Condition 20 relate primarily to trees within the site but not in the area to be redeveloped.
9. The applicant is advised to seek prior consent under s.61 of the Control of Pollution Act 1974 and may request an application from the Environmental Health Team.
10. It is recommended that the applicant liaises with the Town Council regarding the details to comply with Condition 18 as they already have considerable experience of these issues and it is desirable to take a consistent approach with the Market Place
11. With regard to surface water drainage it is the responsibility of a developer to make proper provision for drainage to ground, water courses or a suitable sewer. When it is proposed to connect to a combined public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Connections are not permitted for the removal of groundwater. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. They can be contacted on 0800 009 3921.
12. Where a developer proposes to discharge groundwater into a public sewer, a groundwater discharge permit will be required. Groundwater discharges typically result from construction site dewatering, deep excavations, basement infiltration, borehole installation, testing and site remediation. Groundwater permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 8507 4890 or by emailing [wwqriskmanagement@thameswater.co.uk](mailto:wwqriskmanagement@thameswater.co.uk). Application forms should be completed on line via [www.thameswater.co.uk/wastewaterquality](http://www.thameswater.co.uk/wastewaterquality). Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991.

13. Thames Water recommends the installation of a properly maintained fat trap on all catering establishments. We further recommend, in line with best practice for the disposal of Fats, Oils and Grease, the collection of waste oil by a contractor, particularly to recycle for the production of bio diesel. Failure to implement these recommendations may result in this and other properties suffering blocked drains, sewage flooding and pollution to local watercourses.
14. A Trade Effluent Consent will be required for any Effluent discharge other than a 'Domestic Discharge'. Any discharge without this consent is illegal and may result in prosecution. (Domestic usage for example includes - toilets, showers, washbasins, baths and canteens). Typical Trade Effluent processes include: - Laundrette/Laundry, PCB manufacture, photographic/printing, food preparation, abattoir, farm wastes, vehicle washing, metal plating/finishing, cattle market wash down, chemical manufacture, treated cooling water and any other process which produces contaminated water. Pre-treatment, separate metering, sampling access etc, may be required before the Company can give its consent. Applications should be made at <http://www.thameswater.co.uk/business/9993.htm> or alternatively to Waste Water Quality, Crossness STW, Belvedere Road, Abbeywood, London. SE2 9AQ. Telephone: 020 3577 9200.

#### RELEVANT PLANNING HISTORY

F/2002/6941 & CA/2002/6942	<p>9 July 2003: <b>resolution to grant</b> conditional planning permission and conservation area consent for redevelopment of land at Peach Street, Market Place and Rose Street, to provide 19 units for town centre uses, 64 dwellings (60 flats plus four houses) and an underground car park.</p> <p>The approval was subject to a S106 agreement, which was never completed and, therefore, planning permission was not issued.</p>
F/2005/5294 & CA/2005/5295	<p>Revised proposal for redevelopment of land at Peach Street, Market Place and Rose Street submitted but <b>not proceeded with</b>.</p>
F/2006/8307 & CA/2006/9011	<p>29 November 2006: <b>resolution to grant</b> conservation area consent and conditional planning permission for redevelopment of 1.1 hectares of land at Peach Street, Market Place and Rose Street to provide 27 units/7,352m<sup>2</sup> for town centre uses (a net reduction of 239m<sup>2</sup>), 142 dwellings (137 flats plus 5 houses) and a decked car park.</p> <p>The S106 agreement was never completed and, therefore, planning permission was not issued.</p>
SO/2012/1043	<p>12 June 2012: a screening opinion established that combined proposals for redevelopment of "Peach Place" and development at Elms Field constituted Environmental Impact Assessment (EIA) development.</p>
SO/2012/1044	<p>19 July 2012: a Scoping Opinion to establish the content of an EIA for the proposed "Peach Place" and Elms Field</p>

	developments.
F/2012/1678 CA/2012/1753	Refurbishment of the corner of Market Place and Rose Street (Part of the “Peach Place” site) conditionally <b>approved</b> , following the resolution of the Planning Committee on 14 November 2012.
C/2013/1288	18 June 2013: submission of details to comply with conditions of planning permission F/2012/1678.
F/2013/2283	20 December 2013: full application for redevelopment of 0.95 hectares of land at Rose Street, Market Place and Peach Street to provide a 8,123m <sup>2</sup> development incorporating 6,972m <sup>2</sup> for town centre uses and ten dwellings (a net gain of four) as part of a comprehensive Town Centre Regeneration Scheme (linked with the Elms Field development) <b>withdrawn</b> prior to determination.
F/2013/2284	20 December 2013: full application for development of 6.33 hectares of land at Elms Field and the Paddocks Car Park to provide a mixed use development of 47,133m <sup>2</sup> incorporating 13,475m <sup>2</sup> for town centre uses, a 8,169m <sup>2</sup> food store, an 3,504m <sup>2</sup> hotel, 474m <sup>2</sup> for community use or ancillary retail, 151 dwellings, with associated access and parking (some underground) and public realm works as part of a comprehensive Town Centre Regeneration Scheme (linked with the Peach Place development) <b>withdrawn</b> prior to determination.
VAR/2013/1174	26 July 2013: application to vary condition 11 of planning consent F/2012/1678 to allow changes to working hours <b>approved</b> .
NMT/2014/0104	25 February 2014: application for a non-material amendment to planning permission F/2012/1678 <b>withdrawn</b> . (The proposed increase in the overall height of pitched roof on the corner of Rose Street and Market Place was incorporated in VAR/2014/0453).
VAR/2014/0453	23 May 2014: application for a variation of Condition 2 of planning permission F/2012/1678 (approved drawings) to clarify the roof form of 42 Market Place <b>approved</b> .
SO/2014/1769	26 August 2014: a screening opinion established that the proposals for redevelopment of “Peach Place” alone do not constitute EIA development.
VAR/2014/1496	14 August 2014: variation of Conditions 2 & 10 of F/2012/1678 to allow use of No 38 as a dentist or alternatively residential use at first-floor <b>approved</b> .

### SUMMARY INFORMATION

Site Area	1.09 hectares				
Land uses and floorspace	Existing	Retained	Proposed new build	Total Proposed	Net change
A1 shops	5,369m <sup>2</sup>	3,682m <sup>2</sup>	N/A	3,682m <sup>2</sup>	-1,687m <sup>2</sup>

A2 financial and professional services		0m <sup>2</sup>	0m <sup>2</sup>	N/A	0m <sup>2</sup>	0m <sup>2</sup>
A3 restaurants and cafes		75m <sup>2</sup>	0m <sup>2</sup>	N/A	0m <sup>2</sup>	-75m <sup>2</sup>
A4 Drinking Establishment		258m <sup>2</sup>	258m <sup>2</sup>	N/A	258m <sup>2</sup>	0m <sup>2</sup>
A5 hot-food take away		200m <sup>2</sup>	200m <sup>2</sup>	N/A	200m <sup>2</sup>	0m <sup>2</sup>
Flexible A Class use		N/A	N/A	3,674m <sup>2</sup>	3,674m <sup>2</sup>	+3,674m <sup>2</sup>
<b>Total A Class uses</b>		<b>5,902m<sup>2</sup></b>	<b>4,140m<sup>2</sup></b>	<b>3,674m<sup>2</sup></b>	<b>7,814m<sup>2</sup></b>	<b>+1,912m<sup>2</sup></b>
B1(a) office (demolished)		482m <sup>2</sup>	0m <sup>2</sup>	0m <sup>2</sup>	0m <sup>2</sup>	-482m <sup>2</sup>
D1 non-residential institution (dentist)		161m <sup>2</sup>	0m <sup>2</sup>	0m <sup>2</sup>	0m <sup>2</sup>	-161m <sup>2</sup>
Public Toilet		66m <sup>2</sup>	0m <sup>2</sup>	17m <sup>2</sup>		-66m <sup>2</sup>
<b>Total</b>		<b>6,611m<sup>2</sup></b>	<b>4,140m<sup>2</sup></b>	<b>3,691m<sup>2</sup></b>	<b>7,831m<sup>2</sup></b>	<b>+1,221m<sup>2</sup></b>
Residential	one-bedroom flats		1	1 refurbished flat (4%)		0
	two-bedroom flats		4 (one at 18 Peach Street to be demolished)	21 (three refurbished and 18 new build) (81%)		+17
	Three-bedroom houses		0	4 new build (15%)		+4
<b>Total residential</b>			<b>5</b>	<b>26</b>		<b>+21</b>
Number of affordable units proposed		None				
New civic space		1,250m <sup>2</sup>				
Number of jobs created/lost		Existing		Proposed		Net Change
		53		160		+107
Cycle Parking	Proposed	7 secure, enclosed spaces for staff 30 short term for visiting members of the public Two secure, enclosed spaces for each dwelling				
car parking		Existing		Proposed		Net Change
	M&S	58		58		0
		of which three are used by the Methodist Church				
	Rose Street, Short-stay shoppers' spaces	43		0		-43
	Rose Street, privately let on	56		0		-56

	short-term leases			
	Residential	0	0	0
	Total	157	58	-99

## CONSULTATION RESPONSES

Land Use and Transportation Team	<p>The proposed delivery of a mix of uses in the application site - with retail as the dominant use - accords with Development Plan policy and the Town Centre Masterplan.</p> <p>Retail uses should be sought in primary shopping frontages (Peach St and the Arcade) but a more flexible approach is appropriate elsewhere in the development (Condition 3). An employment skills plan should also be secured (Condition 43).</p>
Access and Movement	No objection subject to conditions (conditions 22-28)
Drainage	No objection subject to conditions (Condition 39 )
Waste	No objection: the proposal provides for separate, secure storage of household and business waste and access for collection of waste; early engagement to ensure that the on-site toilet provision fits into the “Local Loo” service is encouraged ( <i>Officer Note: the applicant has already done this. The WC is intended to be operated by the management company for the development</i> ).
Public Right of Way	No comments received
Conservation Officer	<p>No objection subject to conditions to protect the historic structure of 36 Market Place and to secure details further details of materials, brickwork and the detailing of various elements of the buildings (Conditions 11-16 &amp; 18).</p> <p>Views of the rear of the former stable range of the Bush Hotel, now forming Bush Walk, and of the town hall beyond, would be largely lost: these visual links to the historic townscape provide a strong sense of place. While regrettable this does not amount to a reason for refusal or outweigh the positive benefits. (<i>Officer Note: the applicants have indicated that it may be possible to address this – see paragraph 55 – and a condition is recommended (Condition 12).</i>)</p>
Archaeological Advisor	No objection: recommends that a preliminary phase of an exploratory field evaluation (as set out in the submitted WSI) is undertaken prior to determination but, failing that, works should be secured by Condition (Condition 36).
Landscape Officer	No objection subject to a condition to secure protection of retained trees and implementation of a landscaping scheme, including appropriate tree pits (Condition 18-21).
Ecology	No objection subject to conditions to secure implementation of

	the recommendations of the submitted bat report and a scheme of swift nesting boxes (Conditions 37-38). Also the contributions towards mitigation of the impact upon the Special Protection Area (see the recommendation and paragraphs 135-137 )
Community Infrastructure	To meet the requirements of Core Strategy Policy CP5, a minimum of 30% of the residential units (net) should be affordable housing, which equates to 6.3 units, to be provided as a commuted sum of £637,000 (index-linked) towards off-site provision. (See paragraphs 151-153).
Health and Wellbeing Commissioning Team	No comments received
Partnership Support	No comments received
Environmental Health Officer	No objection subject to conditions 28-35.
English Heritage	<p>No objection: the proposals are a revision to the previous scheme and have been developed in consultation with the LPA and English Heritage. <i>“...the plans respond well to the existing character of the town centre and would be an improvement on what is currently on the site. The design of the Rose Street houses now strikes an appropriate balance between traditional and contemporary styling and the lowering of part of the Peach Street frontage to two storeys plus a mansard reduces the scale of the development and adds a bit of much needed variety to the roof line. The ... three gabled frontage works well, though the gables do look a bit tall and it would be worth looking into whether it was possible to drop the roofline slightly.”</i></p> <p><i>(Officer Note: In response to the last point, the applicants explained that it is anticipated that the retail unit SU006 will trade from both levels (requiring a clear height of 3,500mm each) and the proposed roof pitch of 52° has been chosen to reflect the local vernacular: it is the same pitch as the Methodist Church and others in the area are up to 59°. Therefore, it is not proposed to revise the scheme and English Heritage has confirmed that this on its own would not warrant refusal of the application.)</i></p> <p><i>“The success of all these buildings will be dependent on very high quality brickwork.”</i> (Conditions 8-13 refer).</p>
Environment Agency	The application should be assessed using the Environment Agency’s standing advice.
Natural England	No objection: the site lies on the boundary of 5km from the Thames Basin Heaths Special Protection Area (TBHSPA) and any net gain of Class C3 dwellings with their access point / front door within 5km of the SPA should pay SANG and SAMM contributions in accordance with Wokingham’s TBH strategy. (See the recommendation and paragraphs 135-137 )
Sport England	Does not wish to comment on the application.

Fields in Trust	No comments received
Crime Prevention Design Advisor	No comments received
Thames Valley Police	No comments received
Royal Berkshire Fire and Rescue	No objection: there is a possible requirement for hydrant provision (Condition 42); access for firefighting, including a clear minimum 3.1metre opening for any emergency access gates, should be in accordance with Building Regulations.
Thames Water	No objection on sewage infrastructure grounds.  There are public sewers crossing the site and – to ensure suitable access for maintenance - approval from Thames Water will be required for building in proximity to them (which may be withheld). <i>(Officer Note: the applicants' intention is to divert the sewers, the cost of which has been taken into account in their cost plan).</i>  Informatives are recommended regarding discharge of surface water and ground water, fat traps and trade effluent (Informatives 11-14)
Southeast Water	No comments received
NHS Wokingham Clinical Commissioning Group	No comments received
Highways Agency	No objection
Network Rail	No objection to the current proposal: the Council has worked with Network Rail in relation to stations improvements and the link road and should continue to do so.
Southwest Trains	No comments received
Wokingham Town Council	Consider the proposal an improvement on the previous application but still have reservations about aspects of the design of the buildings, the lack of a comprehensive parking strategy, access, servicing and provision for events, and the retail mix. Full comments are attached.  In summary:  2.1, height of the triple gable <i>(Officer Note: see the English Heritage comments above and paragraph 59)</i>  2.2, detailing of the brick gables <i>(Officer Note: see paragraphs 59-60 and Condition 13)</i>  2.3, brick colour <i>(Officer Note: coloured drawings can give an indication but never give an accurate representation of materials. As is usual practice Condition 7 requires material samples to be submitted for approval. As with the first phase of the regeneration, bricks will be expected to be a local colour)</i>

2.4, detailing of elevations (*Officer Note: paragraph 57 refers*)

2.5, pitched roofs, even if only slightly raised, would be preferable in Peach Place. (*Officer Note: some shallow pitched roofs have been incorporated in the scheme (over SU005 and SU013). Varied roof forms are a feature of the town centre and many of the older (Georgian) properties have - or appear from ground level to have – flat roofs, so a proportion of flat roofs would not be out of character. Furthermore, they provide a location for the discreet siting of the plant associated with town centre uses and PVs – see paragraphs 64 & 150*)

2.6 & 2.7, materials and roof form of the houses (*Officer Note: slates are commonly used in the town centre if not in Rose Street itself. It would be a short terrace of four houses, 23 metres in width, not dissimilar to others in the street where terraced forms with horizontal emphasis are common (see paragraph 49)*)

2.8, dormers, (*Officer Note: see paragraph 63 and Condition 13*)

2.9, pigeons. (*Officer Note: while it is accepted that pigeon perching is an issue this comment does to some extent contradict the request for relief in the elevations but will be given consideration at the detail design stage.*)

3.1- 3.3, absence of a comprehensive parking strategy for Wokingham Town (*Officer Note: see paragraph 115*).

3.4 Question the reliance on the parking standards in the MDDL and how they have been applied to the proposal (*Officer Note: the MDDL is a planning policy document, adopted following statutory consultation and the standards within it are the appropriate starting point for assessment of the application. See paragraphs 112-124*)

3.4.10 Easthampstead Road Car Park is over 250m from the centre (*Officer Note: the car park lies within the designated town centre and the entrance to pedestrian access is adjacent to the primary shopping area which extends as far east as 87 Peach Street*)

3.4.11-3.4.13, available parking capacity in the town centre (*Officer Note: see paragraph 120*)

3.5.2, need to improve the Elms Road multi-storey car park above Argos (*Officer Note: this is beyond the scope of this application but the Borough Council is actively seeking to secure improvements to this car park*).

3.5.3, lack of residential parking (*Officer Note: see paragraphs 122-124.*)

3.5.4 increased congestion due to difficulty of residents on the north side of the town accessing parking which is predominantly in the south.

3.5.5, there will be no provision for residents' parking in Rose Street (*Officer Note: The TRO for the Rose Street residents parking limits eligibility to those living in Rose Street and, as the*

*scheme is already oversubscribed it is unlikely that new applications would be successful.)*

3.5.6, the additional shoppers that will be brought to the town from the SDLs have been ignored (*Officer Note: the infrastructure packages to support the SDLs include measures to encourage sustainable travel and reduce reliance on the private car ; for example improved cycle link to the town centre. Nevertheless, there will be additional car trips which will be taken into account in the Car Park Management Plan.*)

4.2, Cycle parking provision. (*Officer Note: the proposal would provide cycle parking for residents, staff and visitors in line with adopted standards. See paragraphs 125-128 and Condition 27).*

4.1, 4.3, 4.4, improvements in Wilshire Road/Rectory Road, Market Place and Peach Street. (*Officer Note: these suggestions are beyond the scope of this application. However, a working group has been established, with representatives from the Town and Borough Councils, to identify and facilitate a phased programme of deliverable town centre environmental improvements, such as those mentioned. These projects are likely to be funded largely through developer contributions including the current application: see p158)*

5, control over servicing (*Officer Note: see paragraphs 102-. Servicing of units outside the application site boundary – except those in Phase 1 of the regeneration which are in the same ownership and share serving facilities with the application site – is beyond the scope of this application. Planning conditions will secure the servicing facility and a management plan, which the Council - as landlord – will also be able to enforce through lease arrangements but it is beyond planning powers to prevent legal use of the highway. Facilitating/controlling servicing can be considered further in conjunction with town centre environmental improvements.*)

6, events (*Officer Note: condition18 and informative 10 refer*)

7.1, could consideration be taken to encouraging Boots to take a second floor and fulfil an anchor store role? (*Officer Note: this is beyond the scope of this application. Nevertheless, Boots does already occupy two floors although their sales area is confined to the ground-floor. The store does already fulfil something of an anchor role, although anchor stores are generally located at the extremities of the retail circuit in order to encourage footfall)*

7.2, has sufficient consideration been given to changing patterns of shopping (*Officer Note: see paragraphs 22-35)*

Ward Members  
(Wescott)

No comments received

Adjoining Ward  
Members  
(Ewendons,  
Emmbrook,

No comments received

Norreys)

Bracknell Forest Borough Council (adjoining authority) No comments received

Reading Borough Council (adjoining authority) No objection

## REPRESENTATIONS

82 representations have been received: of these 10 are in support of the application and the remaining 75 are objections. The objections include letters from the Rose Street Residents Society, Great Langborough Residents Association, the Wokingham Society, Wokingham History Group, Wokingham Methodist Church, Wokingham CLASP (Caring, Listening and Supporting Partnership) and Cllr Prue Bray, the Leader of the Opposition. The main issue raised by objectors is the loss of short-stay town centre parking: a proportion of them are supportive in principle but have concerns about specific aspects of the scheme.

In addition two petitions have been received objecting to the proposal: these are considered separately at the end of this section of the report.

The comments made in support of the application are summarised below:

- sounds like a great idea;
- pleased to see The Redan and Haka retained;
- the pedestrianised courtyard will be an asset for the town providing a rare quiet space away from traffic and a focal point for events as well as encouraging longer dwell times and footfall;
- the town centre will be enhanced by the provision of a greater choice of quality shops, coffee shops, services and other day time activities as well as new dwellings;
- the new public toilet is also supported;
- the scheme looks good, is interesting and in harmony with the rest of the town centre. Brick piers between the shops makes their appearance more interesting
- Removal of the columns along Peach Street will reduce pavement clutter and enable front of buildings to be brought forward (*Officer note: the intention is to provide more space for pedestrian circulation, rather than bring the buildings forward*)
- The long-term gains will outweigh the disruption during the build (from a local retailer)
- Loss of less than 100 spaces in a town centre with so much parking hardly seems an issue. A resident from near the Cock Pit car park commented that their visitors park there without difficulty and they doubt this will change.
- The car parks would benefit from better signage (*Officer Note: this may be something that can be taken forward in conjunction with town centre environmental improvements.*)

In summary the objections are:

*Officer Note: A number of comments summarised below include suggested amendments to the scheme. The application must be assessed on its own merits and the fact that there are alternative – potentially preferable – options is not a reason to withhold planning permission for an otherwise acceptable scheme.*

#### Need

- There is no need for additional retail units with many already vacant and the increase in on-line shopping. There are already enough restaurants and cafes. (*Officer Note: see paragraphs 22-36*)

#### Character and amenity

- The proposal is not in keeping with historic character of Wokingham. The scale and height of the proposed buildings (in particular the apex of SU006) is out of proportion with surrounding buildings and will be overbearing/impose on Peach Street/cause overshadowing. The proposed design of the buildings is homogenous/generic/ bland/slab like/uniform/reflects the 1960's buildings they are to replace, with repetitive buildings of the same design and too many straight lines: more variety of building lines, roof lines, gables, dormer windows and shop windows is needed to provide more interest. Some correspondents find the rendering of SU001 and SU002 too austere while others consider the brick should be broken up with white walls. (*Officer Note: the scheme design is considered in paragraphs 37-64 of this report*).
- The buildings on Rose Street are unsympathetic towards the Methodist Church/ Bradbury Centre will reduce light and restrict views of the Bradbury Centre - a focal point in the town centre - from Bush Walk. SU013 is massive with no discernible architectural features. It would be a better to open the plaza onto Rose Street and place additional retail units behind the church to block out the ugly M&S building and improve the amount of light entering the square (*Office Note: A more or less continuous line of buildings, close to the back edge of the pavement is typical of Rose Street and the proposal seeks to 'mend' the uncharacteristic gap. As the street lies to the north of the site the omission of the buildings from the Rose Street frontage would not reduce shadowing of the square*).
- Suggest green wall to 'hide' the back of M&S (*Officer Note: no alterations are proposed to M&S at this stage although it is hoped that they may wish to carry out improvements to the rear of the store in future: indicative alterations have been illustrated*).
- While the design of the proposed houses on Rose Street has been improved their scale is unacceptable; they remain too tall and too chunky in design detail; the uniform ridgeline of the slate roof, disproportionately large dormers and aluminium windows do not reflect the varied character and fine detail of local buildings; they could be made double aspect to avoid the need for front dormers; there is insufficient amenity space and parking for local residents. (*Officer Note: the design of these properties – which have dormers front and rear already - and future residents' amenity are considered in paragraphs 61-63 & 77*).
- It would be better for the proposed houses to be commercial units to form a logical retail block.
- Could more buildings be linked? (*Officer note: the terraced form of the buildings*

*provides an appropriate balance between enclosure of the streets, as you would expect in a town centre and pedestrian access through the development)*

- The colonnade provides shelter in bad weather and glass rain protection should be provided where possible.
- The square will provide a positive central rendezvous point but the height of the surrounding buildings will make the space feel quite small/ shaded/disconnected from the town and the design is lacking in imagination. *(Officer Note: the buildings around the square will be two-storey and will provide a degree of enclosure, which is necessary to provide an intimate space – see paragraph 53- without being overly dominating.)*
- The creation of Peach Place Piazza will detract from the historic centre of Wokingham around the town hall. *(Officer Note: the Wokingham Town Centre Masterplan SPD establishes that there should be a series of public spaces in the town centre, each with a different character and function. The document identifies the opportunity to provide a smaller, more intimate and peaceful courtyard within the development to compliment rather than compete with the Market Place, which will remain at the heart of the town. The Town and Borough Council are collaborating to bring forward environmental improvements in the Market Place which will help it consolidate this role within the public space hierarchy)*
- Views, in particular of the undulating roofline of Bush Walk and the Town Hall spire and clock – a focal point - would be reduced and the scheme should be revised to retain these views. *(Officer Note: see paragraph 54 and Condition 12 )*
- Different materials and brick detailing should be used to provide more variety; use of hand-made bricks and lime mortar is suggested *(Officer Note: the detailed elevations show the intention is to use brickwork and other detailing of the elevations to provide interest. However, it is important that the level of detailing is appropriate to the relatively low key, market town character of the town and does not compete with unique buildings like the town hall which should continue to stand out as a special, landmark building. Conditions 7-11 require approval of materials and detailing of the buildings, which will be expected to be high quality. Nevertheless, use of handmade bricks and lime mortar would be an excessive requirement for what will be a 21<sup>st</sup> century development).*
- SU12 would be better brick than rendered *(Officer Note: this is an extension to the refurbished 1-5 Rose Street and a rendered finish is the best way of blending in the alterations to the existing elevation -changes in window proportions for example) and the extension).*
- Windows should be white rather than grey and have a more traditional sash or multi-paned appearance.
- The opportunity should be taken to increase the width of the pavement along Peach Street and remove pinch points; a further increase in pavement width would allow space for cafes and planting: *(Officer Note: the removal of the colonnade will reduce clutter on the pavement and improve pedestrian circulation. Future planned environmental improvements will provide an opportunity for more comprehensive improvements to the pedestrian environment along Peach Street; although this is unlikely to be sufficient for significant pavement seating. The new square will provide opportunities in a quieter location.)*
- Trees, planting and tasteful street furniture are needed. Also more outdoor seating for cafes and restaurants. *(Officer note: the new square provides an*

opportunity for outdoor seating in a relatively peaceful location. Condition 18 refers)

- There is an opportunity for public art to be located in the square (*Officer Note: given the financial constraints of this project it is preferable to invest in the quality of the buildings and public realm at this stage but that would not prevent artworks being installed at a later date*).
- 18-22 Peach Street have traditional character/ interest and are capable of refurbishment. (*Officer Note: see paragraph 44*).
- Loss of the wall on Rose Street (*Officer Note: paragraph 45 refers*).
- Loss of the Arcade: (*Officer Note: the Arcade is part of the unsympathetic 1960's development and has a negative impact on the character of the area (see paragraph 43). The proposal will provide alternative retail accommodation to meet the needs of the town. See paragraphs 23-33*)
- The site slopes but the CGI appears to show whole site as being level, which has implications for building heights. (*Officer Note: there is only a 0.5 metre difference in levels across the site (DAS paragraph 3.1.1) which is not significant; the public space has been designed to be level and free from steps and the shops are intended to have flush thresholds (DAS p7.8.4). Condition will confirm levels.*)
- A-boards are liable to blow over in wind (*Officer Note: signage is the subject of a separate system of advertisement consent. Nevertheless, the approach to signage is important to the success of the town centre and a signage strategy is proposed; Condition 17*).
- The passageway beside Costa Coffee should be kept open (*Officer Note: this is a narrow passage and not overlooked. The scheme provides good pedestrian connectivity with wider paths that are overlooked by surrounding buildings providing natural surveillance; these will provide more attractive, safer routes*)
- Can cabling, condensers and other services be banned from view? (*Officer Note: the scheme has been designed to accommodate A class uses including restaurants/ takeaways and possible future need to install equipment discretely. Ducting routes have been designed in and screened plant areas identified on the roof of the buildings paragraph 64 & condition 34 refer* )
- Refurbished 'Clark's Corner' building will overshadow the Rose Street buildings opposite (*Officer Note: the building on the corner of Market Place and Rose Street is outside the application site boundary and does not form part of the current proposals*).

#### Access, parking and servicing

- The proposals do not mitigate the impact upon the local transport network. (*Officer Note: this is considered in paragraphs 99 and 154-160* )
- The proposal would result in loss of short term parking (including disabled bays) which is used by visitors to the Bradbury Centre (47% of over 80,000 users per annum) and Wokingham Medical Centre (many of whom are elderly) and lack of parking for residents. The M&S disabled bays will not be sufficient to meet demand. The new homes and businesses will generate demand for additional parking and lack of convenient parking will deter visitors. Are there plans to create additional places? The proposal would not provide parking in line with the Council's adopted standards, the assumptions about parking demand and data

about utilisation of town centre car parks are flawed and the alternative car parks are not convenient particularly for residents, the elderly and disabled. The disabled bays should be replaced in Rose Street. (Officer Note: this is considered in paragraph 117 and Condition 26 refers )

- Units SU014, SU015 and SU016 should be omitted to allow retention of some parking on the site. (*Officer Note: as explained at the start of the summary of objections, the application must be assessed on its own merits.*)
- Wokingham Medical Centre was permitted without patient parking due to reliance on Rose Street car park. (*Officer Note: the parking provision available at the Medical Centre includes four disabled bays and a drop-off point, the Centre was already operating with very limited parking at their previous premises and they were able to demonstrate a high level of sustainable travel and an accessible location; there was no specific reliance on any particular town centre car park.*)
- The proposal may increase servicing on Peach Street, which already causes disruption. Service Access will be from Broad Street causing vehicles arriving via London Road to travel around the town centre, increasing congestion, rather than using Cross Street. How will delivery lorries be routed as Rose Street is narrow. Large goods vehicles should be banned from Rose Street. (*Officer Note: servicing is considered in paragraphs 102-109. There will not be a significant increase in the number of delivery vehicles or an increase in their size as a result of the proposals and no changes to existing circulation patterns is proposed, so they will still be able to approach from either Broad Street or Rose Street via Cross Street.*)
- The proposed servicing arrangements would restrict drop-off/disabled access to the Bradbury Centre (operational from 08.30 to 22:00 daily); the bollards on the eastern exit should be moved back into the site to allow access; servicing would take place at times when the centre is in regular use by various groups; and the existing safety barrier would be removed. (*Officer Note: the Bradbury Centre has benefited from its position next to a public car park which has enabled visitors to be dropped off at the entrance but there is no formal arrangement in place. The applicants advise that they have had an ongoing dialogue with the Bradbury Centre regarding the feasibility of incorporating a drop-off facility in the scheme: providing access through the square would be difficult to manage and would undermine the objective of providing a safe, pleasant pedestrian environment; allowing access via the servicing egress would result in reversing either in or out which would be detrimental to safety. The applicants have also considered how disabled provision can be re-provided (see paragraph 117). Currently servicing can take place throughout the day, in addition to the considerable number of vehicle movements associated with the use of the car park. The provision of a pedestrian square adjacent to the building would significantly reduce the number of vehicle movements immediately adjacent to the Bradbury Centre entrance improving pedestrian safety even without the wall, which in any case extends onto land outside the Church's control.*)
- How will deliveries to the commercial units be managed to avoid conflict with pedestrians using the square/illegal parking/provide emergency access. It will be difficult to police delivery time restriction. (*Officer Note: paragraphs 102-108 and conditions 22-23 refer*)
- Good lighting will be required to avoid danger to pedestrians when service vehicles are operating (*Officer Note: Condition 18 refers*)

- The Transport Statement suggests that smaller delivery vehicles could reverse onto Rose Street rather than passing through the square, which would be hazardous to pedestrian and vehicular traffic. *(Officer Note: servicing access for smaller vehicles has been reviewed by the applicants and additional details have been submitted demonstrating how vehicles could turn within the site; see paragraphs 102-108).*
- There is no provision for turning at the Broad Street end of Rose Street in order to exit to Wiltshire Road. *(Officer Note: No change to the current situation is proposed. It will remain possible to turn right out of Rose Street at either end - into Broad Street or Wiltshire Road - as is currently the case.)*
- Suggest pedestrian crossings between the site and Old Row Court and across Peach Street for pedestrian safety and to reduce traffic speeds. Also a reduction in to a 20mph speed limit in Peach Street/the Market Place. *(Officer Note: these issues will be considered as part of wider town centre environmental improvements)*
- Little thought has been given to connections between the new square and Bush Walk /Old Row Court *(Officer Note: Bush Walk is outside the application site – although in the applicants’ control – and there are no proposals for alterations to it at this stage. The two end shop units have display windows facing into the car park and the new square would extend up to them, enhancing their setting and integrating the existing arcade with the new development. There are no current proposals to improve the crossing facilities in Rose Street but it is likely that pedestrian connections between the application site and Old Row Court will be incorporated in future town centre environmental improvements.*
- Facilities should be provided for cyclists *(Officer Note: see paragraphs 125-129)*
- The Transport Statement omits to mention that Sunday and evening bus services are limited, making access without private transport difficult. *(Officer Note: measures to support sustainable travel are a Core Strategy objective. While improvements to bus services are beyond the scope of this application alone a thriving town centre is more likely to support frequent, viable bus services).*
- Consideration needs to be given to routing of construction traffic including the potential use of Cross Street and the impact on historic vehicles in Rose Street (with minimal foundations). Also to maintaining safe access to the Bradbury Centre throughout the development period. *(Officer Note: Condition 28 refers)*
- More residents’ parking spaces (with permits) are required for existing Rose Street residents. *(Officer Note: the current proposal would reduce short stay parking (due to re-provision of disabled parking in Rose Street) but would not alter the availability of residents parking spaces; the application cannot be expected to resolve existing issues that do not relate directly to it)*
- The Bradbury Centre has established a right of access. *(Officer Note: this is a legal matter rather than a planning matter).*

#### Other issues

- An archaeological investigation should be required before determination. *(Officer Note: paragraphs 130-132 and Condition 36 refer)*
- Will lifts be provided to allow the flats to be occupied by people with limited physical capability? *(Officer Note: lift access will be provided to all the flats except*

for 2B and 3)

- The disabled toilets appear to have two sets of doors, which could prove a barrier to use; will there be baby changing; rain water could be used for flushing toilets; will there be a charge for use? (*Officer Note: the toilets have been designed to accommodate wheelchair use/comply with building regulations for disabled access and will incorporate baby changing facilities; grey water recycling is not proposed; charging is not a planning matter but the applicants have advised that the intention is that the facility will be operated by the development's managing agent on behalf of the Council and that there will be a charge.*)
- There is an opportunity to relay power distribution cables making the substation adjacent to the Bradbury Centre redundant (*Officer Note: this is outside the application site boundary and there are no proposals to remove it as part of the application*)

A 512 signature petition has been received objecting to the demotion of Vitality Health Food, 20 Peach Street “*on the grounds that it will result in the loss of an important amenity to local residents. The shop has provided a valuable service to local residents for the last 30 years (since March 1985) and has contributed to the good health and wellbeing of Wokingham residents. In our opinion the scheme should be reconfigured to allow Vitality Health food Shop to stay, as it is at least as important to the good health and wellbeing of local residents as the Redan Public House, which is located next door.*”

The covering letter acknowledges that Wokingham Borough Council has offered an option on another retail unit in the town centre but raises concerns that the location of the alternative unit would not enable them to sustain a high street presence or, therefore, compete effectively with a well-known nation health food chain in Peach Street.

Officer Note: Vitality Health Food operates from a Class A1 shop and the business could relocate to alternative premises in the town centre.

Although there is no requirement for them to do so, the Council seeks to support business where possible. While not a planning matter, the committee may wish to be aware that that the tenant has been offered alternative, available premises in the town centre (with priority over other applicants who are not being displaced due to the regeneration) which are capable of providing continuity of trading. Proposals for redevelopment of the application site have long been in the public arena and – in anticipation of the redevelopment coming forward and in common with most of the premises within the application site boundary - the current lease provides for early termination.

The objections to the loss of the Redan and the adjoining Haka related primarily (although not entirely) to the loss of a building of historic interest which, while not listed, makes an important contribution to the character of the town. This was the planning reason for retaining the building in the revised proposals, rather than the loss of the business which could also relocate elsewhere in the town centre. See also paragraph 44.

Regenerating the town centre depends of a balance between retention of buildings and other features that make a positive contribution to the character and quality of the historic market town and bringing forward sufficiently comprehensive proposals to provide buildings and public spaces required to support a thriving town centre and make a positive difference. Retention of buildings simply because they are occupied by an existing business is an argument that could be repeated numerous times and would

ultimately undermine the regeneration.

A 668 signature petition has been received from the Bradbury Centre objecting on the grounds that the proposal *“does not adequately address the needs of disabled people to gain access to the main entrance of the Bradbury Centre from the proposed Town Square. This will no longer enable Keep Mobile, other mobility groups or private vehicles to get alongside the building (as at present). Consideration has not been given to those disabled persons who are not able to walk from the parking bays in Rose Street.”*

Officer Note: this issue has been considered in the response to representations above and in paragraph 44 of the report.

## **PLANNING POLICY**

### **Wokingham Borough Core Strategy Policies:**

- CP1: Sustainable development
- CP2: Inclusive communities
- CP3: General Principles for development
- CP4: Infrastructure requirements
- CP5: Housing mix, density and affordability
- CP6: Managing travel demand
- CP7: Biodiversity
- CP8: Thames Basin Heaths Special Protection Area
- CP9: Scale and location of development proposals
- CP10: Improvements in the Strategic Transport Network
- CP13: Town centres and shopping
- CP14: Growth and renaissance of Wokingham Town Centre
- CP17: Housing delivery

### **Managing Development Delivery Local Plan Policies**

- CC01: Presumption in Favour of Sustainable Development
- CC02: Development Limits
- CC03: Green Infrastructure, Trees and Landscaping
- CC04: Sustainable Design and Construction
- CC05: Renewable energy and decentralised energy networks
- CC06: Noise
- CC07: Parking
- CC08: Safeguarding alignments of the Strategic Transport Network & Road Infrastructure
- CC09: Development and Flood Risk (from all sources)
- CC10: Sustainable Drainage
- TB05: Housing Mix
- TB07: Internal Space Standards
- TB08: Open Space, sport and recreational facilities standards for residential development
- TB09: Residential accommodation for vulnerable groups
- TB12: Employment Skills Plan
- TB15: Major Town and Small Town/District Centre development
- TB16: Development for Town Centre Uses
- TB19: Outdoor advertising

- TB20: Service Arrangements and deliveries for Employment and Retail use  
 TB21: Landscape Character  
 TB23: Biodiversity and Development  
 TB24: Designated Heritage Assets (Listed Buildings, Historic Parks and Gardens, Scheduled Ancient Monuments and Conservation Areas)  
 SAL05: Delivery of avoidance measures for Thames Basin Heaths Special Protection Area  
 SAL08: Allocated Mixed Use Sites

### **Supplementary Planning Documents and other guidance**

Wokingham Borough Council Borough Design Guide Supplementary Planning Document (June 2012)

Wokingham Town Centre Masterplan Supplementary Planning Document (April 2010)

Sustainable Design and Construction Supplementary Planning Document (May 2012)

Affordable Housing Supplementary Planning Document (July 2013)

Planning Advice Note, Infrastructure Impact Mitigation, contributions for New Development

Wokingham Town Centre and Langborough Road Conservation Areas Study

National Planning Policy Framework and Planning Practice Guidance

## **PLANNING ISSUES**

### The principle of development

1. Managing Development Delivery Local Plan (MDDLDP) Policy CC01 establishes that planning applications that accord with Development Plan policies will be approved unless material considerations indicate otherwise.
2. The application site is situated in the centre of Wokingham, a Major Development Location offering a wide range of facilities and services together with a choice of modes of transport to access them, and the town is capable of accommodating major new development (Core Strategy Policy CP9 and MDDLDP Policy CC02).
3. The proposal would result in a net gain of town centre and residential uses plus a new public open space; a net loss of a surgery, offices and parking; and it would entail demolition in a Conservation Area. The acceptability, in principle, of each element is considered below.

### Town centre uses

4. The site lies within the designated Town Centre and also within the Primary Shopping Area of Wokingham (Core Strategy Policies CP13, CP14 and MDDLDP Policy TB15). These policies identify it as a Major Town Centre (the only one in the Borough), capable of accommodating town centre uses (these include retail, entertainment, arts and culture, indoor recreation, leisure, health, community and office uses) and suitable for growth. Moreover, Policy CP14 establishes an aspiration to rejuvenate the Town Centre: proposals should both retain and enhance the historic character of the town and maintain its position in the Berkshire retail hierarchy by:

- 1) Strengthening shopping in the retail core to reduce leakage of expenditure;
  - 2) Conserving and enhancing historic quality and interest;
  - 3) Improving existing public space; and
  - 4) Ensuring development cumulatively provides and maintains a range of town centre uses, facilitates access them by a variety of modes of travel and achieves enhanced environmental and design quality.
5. These Core Strategy Policies are amplified by the Wokingham Town Centre Masterplan SPD, which establishes a strategy for the regeneration of the town, and MDDL Policy SAL08 which, consistent with the SPD, identifies specific sites which will contribute to the delivery of the strategy: the application site is allocated for a mixed use development for A1 (retail) use, with flexibility for A3 (restaurants & cafes), A4 (drinking establishments) and C3 (residential) development.
6. Consistent with these policies, the application proposes a mixed use development of 7,814m<sup>2</sup> (a net gain of 1,912m<sup>2</sup>) for A Class uses and 26 dwellings (a net gain of 21) and a new public square. The net gain of A Class uses would be over 500m<sup>2</sup> but because the site lies within the defined town centre does not trigger a requirement for a sequential test or retail impact test under MDDL Policy TB16.

#### Residential Use

7. While the majority of the housing delivered in the Borough during the plan period will be within the four Strategic Development Locations, Core Strategy Policy CP17 establishes that approximately 550 new homes will be on identified sites within Major Development Locations and MDDL Policy SAL09 identifies residential use as one of the uses that may be appropriate as part of the mixed use development of the Peach Place site.
8. There are five existing flats within the application site boundaries: one on the second-floor of 18 Peach Street, above the dentist; two on the second floor of 1-5 Rose Street (above Boots Opticians and Strange Jewellers) and two on the first-floor above 36 Market Place (Superdrug and Costa Coffee). Of these one (18 Peach Street) would be demolished but the remaining four would be retained and refurbished. In addition, 22 new dwellings are proposed, resulting in a net gain of 21 (26 in total). Thus, there would be no conflict Core Strategy Policy CP3 i), which resists the loss of residential accommodation.
9. With the exception of the four houses the dwellings would be flats, located on the upper floors, above town centre uses. This is, in principle, consistent with MDDL Policy TB15 which encourages the provision of offices or self-contained dwellings above town-centre uses proving appropriate level of residential amenity can be provided (see paragraphs 69 to 94 for the assessment of amenity) and the Masterplan SPD. The introduction of residential uses on the upper floors is also consistent with the aim of TB15 of providing compatible daytime, evening and night-time uses.

#### Provision of public open space

10. Core Strategy Policy CP3 amplified by MDDL Policy TB08 requires development to provide for a framework of open space, including civic space, and the Masterplan SPD specifically identifies provision of a new town courtyard as one of

the aims of the Peach Place development (Figure 39 and paragraph 12.3.23). The provision of a new square within the development is consistent with this.

Loss of a D1 non-residential institution

11. The first-floor of 18 Peach Street is currently occupied by a Class D1 dental surgery, the Peach Street Dental Practice. Core Strategy Policy CP3 j) resists the loss of community facilities, including health facilities (as defined in paragraph 4.17 of the Core Strategy) unless suitable alternative provision is available.
12. Alternative premises for the dentist have already been secured: conditions of the planning permission for the first phase of the Peach Place regeneration (F/2012/1678) were varied on 14 August 2014 to allow more flexible use of 38 Market Place and the upper floors of 39 Market Place including use as a D1 non-residential institution (VAR/2014/1496). The applicants advise that the terms of the new lease have been agreed. These alternative premises are currently being fitted out and the practice is due to relocate by the end of April 2015.
13. As provision has been made to relocate the surgery, the current proposal would not result in loss of a community facility and there is no conflict with Core Strategy Policy CP3 j).

Loss of Employment floor space

14. The proposal would result in the loss of use of 482m<sup>2</sup> Class B1(a) office accommodation through demolition .
15. Core Strategy Policy CP15 establishes that the change of use of premises from Class B, employment uses should not result in a net loss of floor space in Class B use in the Borough. Provision should be made for a range of sizes, types and qualities to cater for new and expanding businesses and different sectors.
16. The Council's Employment Land Study indicates a need for around 78,000m<sup>2</sup> additional office floor space in the plan period, split between Reading and Wokingham Boroughs. It is anticipated that this will be provided through implementation of existing commitments at Green Park and Thames Valley Park, redevelopment of existing sites in Wokingham and South Reading and new office development in the centre of Reading.
17. The applicants have provided evidence that the Thames Valley office market has suffered as a result of the recession and vacancy rates across the area have increased significantly since the Council's last Employment Land Study in 2006. Take up of office space is down, although the market did rally towards the end of 2014. Supply is polarised between new and relatively expensive grade A office accommodation and poorer quality Grade B offices. Wokingham Town Centre is a secondary office location providing smaller premises, with an over-supply of stock keeping values low. The office accommodate that would be lost is small scale and poor quality. One of the current tenants will be relocating in June this year and the other is a charity, occupying the offices at minimal rent.
18. Another consideration is that office values in Wokingham are low and inclusion of replacement office accommodation within the scheme would impact further on its viability

19. The loss of the relatively small, low quality office space would not materially impact on office supply in the town or wider Borough and would be significantly outweighed by the benefits to the town in terms of its vitality and economy (see paragraph 161).

#### Loss of public and private car parking

20. The proposal would result in the loss of 99 car parking spaces; 43 in the Rose Street short-stay, shoppers' car park and 56 privately let. There is no policy protection for car parking as such, so no in principle objection to its loss, but Core Strategy Policy CP6 does require development to provide appropriate vehicular parking. The impact of the loss of this facility on parking provision in the town as a whole is considered in paragraphs 112-121.

#### Demolition in the Conservation Area

21. The proposals entail demolition of buildings with a volume of more than 115m<sup>3</sup> and also demolition of the wall along the Rose Street frontage, which is over one metre in height and adjacent to the highway. Because the site is within a conservation area these operations require planning permission in their own right: whether demolition is acceptable depends on the contribution the existing buildings make to the historic character of the conservation area and also whether there are proposals in place for an appropriate replacement. These issues are considered in paragraphs 42 to 46 and 37 to 64 respectively.

#### Retail Policy

22. As set out in paragraphs 4-6, the application relates to an allocated site, within Wokingham Town Centre and the proposed town centre uses are acceptable in principle. Nevertheless, consideration should also be given how well the proposed retail provision fulfils the retail policy ambitions for the town.

#### The SPD Vision

23. The objectives of Core Strategy Policy CP14 include strengthening shopping in the retail core of the town to reduce leakage of expenditure and (cumulatively) providing a range of town centre uses. To achieve this, the Masterplan SPD identifies five objectives for the town centre: a thriving high street, an eighteen hour economy, streets as places, spaces to breath and setting the design standard. The limited existing retail core is identified as one of the constraints to the success of the town centre and to achieve these objectives will be necessary to strengthen and extend the retail core, provide new high quality retail development to strengthen the existing offer and encouraging niche and boutique shopping alongside larger retailers.
24. The SPD establishes a land use/retail strategy to delivery this vision for the town centre. It consists of two mixed use retail quarters - one to the north-east of the Market Place, extending along Rose Street and Peach Street with the existing Marks and Spencer store forming a retail anchor; the other at the southern end of Denmark Street centred on a new retail anchor - connected by a high street (Denmark Street). The application site lies within the northern retail quarter, which it is envisaged will provide retail led mixed use development on the ground-floor with residential above. Paragraph 10.2.3 explains "*The retail heart of the town*

*centre at Market Place/ Denmark Street will be strengthened to increase the number of retailers in the town centre and meet modern retailer requirements. Peach Street will become a focus for a range of comparison retailing.... These shops will sit alongside cafes and restaurants, to create a fine grain, mixed use environment with active frontages onto an intimate courtyard space.”*

#### The Council's Retail Study

25. The policy framework for the town centre was informed by a retail study, commissioned in 2007 and refreshed twice since, in 2010 and 2014. The most recent refresh was to inform the appropriateness of the Wokingham town centre regeneration proposals given recent changes in population, the continued effects of the recession on retail expenditure levels and the growth of internet shopping .
26. The report identifies the two most significant changes since 2010 as the slowdown in expenditure growth and the growth in online shopping, although it still remains a relatively low percentage of total retail expenditure. The on-line shopping share of the market is about 12% of retail sales and is predicted to rise to 17.4% by 2020 and 20% by the end of the 2020's. Other changes in this period include smaller store formats for convenience goods; a rise in charity shops and discount retailers (pound shops); and high street comparison retailers looking for bigger units (over 200m<sup>2</sup>).
27. New retail facilities planned in Wokingham town centre (a net increase of 3,546m<sup>2</sup> convenience and 4,614m<sup>2</sup> comparison, including the current proposals and development at Elms Field identified by Policy SAL08), Arborfield and Shinfield will change future shopping patterns. In particular they will reduce the number of people going outside the Borough to shop - one of the key aims of Core Strategy Policy CP14 - and subsequently market shares will change. This will create additional demand for retail comparison floor space (over and above that mentioned): 2,541m<sup>2</sup> gross by 2021 and 7,799m<sup>2</sup> gross by 2026. These projections are significantly lower than in 2010 due to lower expenditure growth (during the downturn and forecast) and on-line shopping taking a greater share of the market but nevertheless demonstrate that there will be unmet demand even after implementation of the current application.
28. As well as reconfirming the need for additional retail development within Wokingham Town Centre, the retail refresh provides guidance on the type of accommodation required. Currently 70% of the units in Wokingham town centre are small units (less than 150m<sup>2</sup> gross) and are not suitable for multiple retailers, although they may still be attractive to independent traders and non-retail services. There is a limited supply of medium (150-250m<sup>2</sup> gross) and large units (over 250m<sup>2</sup> gross) and the recommended strategy for the town is to provide more medium/large (over 150m<sup>2</sup> gross) units.

#### The applicants' approach

29. The applicants' retail advisor (Strutt & Parker) has identified the increasing dominance of regional shopping destinations such as Reading and Guildford as a factor leading to polarisation of lower order towns. The trend is for these traditional, comparison shopping destinations to specialise, focusing on either aspirational or value retailers: those that have not been able to attract aspirational retailers or been willing to go down the value route have stagnated. Affluent

market towns such as Farnham, Marlow and Henley have continue to thrive by being able to provide an evolving mix of aspirational offer of fashion, food/ beverage and leisure, which encourages visitors to stay longer and increases overall spend compared to a retail-only trip.

30. Strutt & Parker concur that, to attract aspirational retailers, there is a need for larger format units (over 200m<sup>2</sup>). The applicants' planning statement advises that national retailers typically require units of 200–300m<sup>2</sup>, rising to 500m<sup>2</sup> for larger branches, with a flexible design that enables them to expand and contract given market conditions and managed by a single organisation. Larger, flexible modern shop units will be attractive to retailers and restaurants which cannot currently find suitable accommodation in the town enhancing the retail and restaurant offer. The correct balance of key national retailers, restaurants and cafes will make the town attractive to visitors which will in turn help sustain local niche and independent retailers.

#### The existing retail provision

31. The proposal would result in the loss of generally small units, retention of a range of unit sizes and provision of new, flexible accommodation capable of providing the larger units for which the need has been identified. With the exception of Clinton Cards (342m<sup>2</sup>) all of the 21 existing units that would be demolished are small, ranging from an average of 30m<sup>2</sup> in the Arcade to 149m<sup>2</sup> (Code, 18 Peach Street) but with a significant proportion being around 115m<sup>2</sup>, typically with net sales areas of around 50m<sup>2</sup>. The units it is proposed to retain provide a mixture of small, medium and larger units: Strange Jewellers (93m<sup>2</sup>), Boots Opticians (200m<sup>2</sup>), Superdrug (313m<sup>2</sup>), Costa Coffee (162m<sup>2</sup>) and M&S (2,914m<sup>2</sup>) plus the Redan (258m<sup>2</sup>) and the Haka (200m<sup>2</sup>).

#### The proposed retail provision

32. The proposed buildings have been designed for flexibility, to allow the size of units to be increased or reduced to adapt to changing needs of retailers and shoppers. SU006 has been designed as a single, larger, two-storey retail unit (1,286m<sup>2</sup>). Units SU010 and SU012 would be small units of 72m<sup>2</sup> and 101m<sup>2</sup> respectively. The other units have been indicatively shown sub-divided into units of 84-307m<sup>2</sup> but would be capable of being split differently: SU001-SU005 combined could provide up to 1,208m<sup>2</sup>, SU007 and SU008 331m<sup>2</sup> and SU013-SU016 up to 425m<sup>2</sup>. Thus, the proposal will provide for the modern, flexible retail units which are currently lacking in the town centre. It is anticipated that this provision will be complemented by future development at Elms Field, where scale in relation to historic buildings is less of a constraint.
33. Retaining flexibility will assist in both attracting occupants initially and by allowing for adapting adaptation to changing market conditions in future. Unduly restrictive conditions would, therefore, be counterproductive.

#### Use of the proposed retail units

34. As well as being in the designated town centre, the site lies within the Primary Shopping area identified by MDDLPTB15. Furthermore, Peach Street, Market Place, Rose Street and the Arcade are identified as primary shopping frontages: proposals are required to show how they retain or increase the provision of Class

A1 shops in Primary frontages. With the exception of 16A Peach Street, the established use of units it is proposed to demolish is retail.

35. The application seeks flexible consent for uses within Class A (Under Class E of Part 3 of the Second Schedule the Town and Country Planning (General Permitted Development) Order 1995 (as amended): for the first ten years, subject to any conditions, the use of buildings within the development would be able to change freely between any use within Class A. The use at the end of the ten year period would become the established use and from that point forward normal permitted development rights for change of use of the buildings would apply. This flexibility would apply to all units within the application site boundary whether retained or new.
36. Flexibility of use is important for the reasons explained in paragraph 32 and will help establish the mix of uses including cafes etc. which will encourage visitors to linger and support a vibrant town centre throughout the day and evening. However, it is also important to ensure that a predominantly retail character is retained in the primary shopping frontages. Hence, it is recommended that the use of the units in Market Place and Peach Street (except for those retained units that already have established use as an A4 drinking establishment or A5 takeaway) is limited to Class A1 shop, with flexibility being allowed for the units around the square and on Rose Street. Condition 3 refers.

The impact upon the character of the area including the setting of listed buildings and character of the Conservation Area.

37. Core Strategy Policies CP1 and CP3 require proposals to maintain or enhance the high quality of the environment and contribute to a sense of place, while avoiding detriment to heritage features. Policy CP14 relates specifically to Wokingham Town Centre and requires development to retain and enhance the historic market town character of the town, conserving and enhancing historic quality and interest and improving existing public space (paragraph 4). The Masterplan SPD reinforces this, requiring new development to achieve the highest quality of architecture and urban design, be sustainable and enhance the market town character of Wokingham.
38. The application site is in a sensitive location, within the Wokingham Town Centre Conservation Area and while none of the buildings within the application site boundary is listed, a number in the vicinity are; most notably 37 Market Place, the Old Bull and Bush (now Bush Walk), which will form one of the pedestrian links to the new public square within the development. The site also forms the setting of the Town Hall (grade II\*) and a number of grade II and II\* listed buildings on the east and west sides of the Market Place and north side of Rose Street.
39. The Planning (Listed Buildings and Conservation Areas) Act 1990 establishes a statutory duty to consider the effect on heritage assets: development should preserve or enhance the character of the Conservation Area and setting of listed buildings. This is reinforced by MDDL Policy TB24.
40. The site is also situated on a Green Route, designated due to the positive contribution made by the trees and other vegetation that line it. Core Strategy Policy CP3, saved WDL Policy WBE4 and MDD DPD Policy CC03 Development

require development to protect and enhance the Borough's Green Infrastructure, incorporating high quality landscaping as an integral part of the scheme.

41. The Masterplan SPD identifies the application site as falling within three Character areas: the Market Place, Rose Street and Peach Street. The Market Place is the historic focus of the town centre with the Town Hall as its centrepiece, while Rose Street retains most historic character with much development from the fifteenth century onwards surviving. All three character areas are characterised by fine grain development with narrow frontages and long, deep plots although in Peach Street much of the pattern has been lost through twentieth century redevelopment which has introduced wider fronted buildings. Buildings are predominantly 2-3 storey in the Market Place and Peach Street, with a larger proportion of two-storey in Rose Street. Variation and animation in the roof-scape due in part to differing storey heights is characteristic of the Market Place while low eaves and a strong horizontal rhythm are more typical of Rose Street. The post-war redevelopment on the northern side of the Market Place extending around to 1-16 Peach Street is identified as a negative feature.

#### Demolition in the Conservation Area

42. As explained in paragraph 21, the proposals entail demolition of buildings with a volume of more than 115m<sup>3</sup> and also demolition of the wall along the Rose Street frontage, which is over one metre in height and adjacent to the highway. Because the site is within a conservation area these operations require planning permission in their own right. The buildings within the site, to be demolished are shaded on the drawing extract right.



Figure 1: demolition (ground-floor)

43. 34-35 Market Place, 2-16 (even) Peach Street and 1-6 the Arcade are a 1960's development. While the siting of the buildings follows the historic pattern and their scale and rhythm is not out of keeping, the design of the buildings – in particular the repetitive projecting fenestration, unchanging flat roof form, deep overhanging concrete colonnade and materials used - are a poor response to the local historic context. They are of little historic interest or architectural merit and are considered to harm the character of the Conservation Area; there is no reason to resist their loss, providing the development that would replace them would meet the test of preserving or enhancing the Conservation Area and there is some certainty that development will proceed following demolition (Condition 6 refers).
44. 18-22 (even) Peach Street are 19th century buildings, and while they do contribute to the character of the Conservation Area their significance is limited and their loss is also acceptable in the context of providing a high quality and coherent new development.



Figure 2: existing Peach Street elevations

45. Also proposed for demolition is a length of 2.5m high red brick wall which fronts Rose Street and returns into the car park. This is built in stretcher bond with a double bullnose capping and is of mid C 20th date. Whilst in reasonable condition and forming part of the established character of this part of the conservation area, the wall is of limited historic or architectural interest and there is no objection to its loss.
46. The Public Toilets in the Rose Street car park have a volume of less than 115m<sup>3</sup> so their demolition does not require specific approval. Nevertheless it is worth mentioning that the toilets are prefabricated structure, in materials that have no local connection and, hence, appear somewhat incongruous. Accordingly there is no objection to the loss of this structure on character grounds.

*The applicant's analysis of the character of the town centre*

47. The applicants' Design and Access Statement (DAS) incorporates a detailed assessment of the town centre in order to identify the defining characteristics of development in the town and also the differences between areas.
48. The medieval street pattern remains largely unaltered, with the focus being Market Place and the Town Hall, which forms a unique centrepiece. The Market Place is characterised by a variety of building types and forms ranging from fifteenth to nineteenth century properties. In Rose Street the predominant architectural form is sixteenth and seventeenth century cottages but the traditional pattern has been broken on the application site, which adjoins less successful twentieth century buildings. The DAS identifies one of the key challenges for the scheme as being to mend this street scene in a way which respects heritage assets and also forms a relationship with the more modern buildings. While Peach Street's historic street form remains, much of its original historic built fabric has been lost, particularly on the eastern side of the street.
49. Buildings are largely a mix of two and three storey throughout the town centre. There is a strong vertical rhythm in Market Place and also in Peach Street, where it is combined with horizontal banding at shop front and roof levels. In Rose Street ridge heights create a more horizontal rhythm. In Peach Street and Rose Street roof lines are relatively uniform with horizontal eaves lines and parapet details broken by the occasional gable. Further south, in particular Denmark Street there is a more varied roof line, with regular gable ends and varying pitches.
50. Windows are typically generous with a strong vertical arrangement and regular rhythm of windows across the facades. A frequently occurring feature is the bay window at first floor which helps to give interest at upper levels. Dormer windows within the roof are also a feature, particularly along Rose Street.
51. There is a varied palette of materials - a mix of brickwork, painted brickwork,

render and tiling - and period detailing owing to the varying age of buildings. On the whole buildings are executed in a single material but without long stretches of the same material.

52. This analysis has informed the development proposals.

*“The Plaza”: a new public square within the development*

53. One of the objectives of the Masterplan SPD is to build on the existing character of the town (Bush Walk, Old Row Court, Central Walk etc.) by providing a series of small courtyards and arcades connected by a network of pedestrian routes. The application site is identified as providing an opportunity to strengthen the retail offer in the heart of the town by providing small shops, possibly with a larger anchor store. This is envisaged as an intimate public space, framed by active frontages to encourage activity and natural surveillance (Masterplan SPD, paragraphs 4.3.9, 4.3.16, 8.5.10, 10.2.3 & Figure 39). Consistent with this, a new square is proposed at the centre of the development, surrounded by retail units, with flats around on the southern side. It is anticipated that this location will be attractive to cafes and restaurants as it will be a relatively quiet location, away from traffic, with space to provide outdoor seating. The pedestrian links into the square have also been designed as active frontages to integrate them into the development and fulfil the requirement of Core Strategy Policy CP3 for proposals to be functional, accessible, safe and secure.

54. Core Strategy CP3 and MDDLP Policy CC03 require protection of important landscape features and provision of new green infrastructure. The few trees that there are on the site are not identified as being high quality or significant and there is no objection to their loss, particularly as the proposed scheme offers the opportunity for planting a number of larger specimens as part of the new square. The DAS establishes appropriate principles for the new square, which are consistent with the Masterplan objectives. Condition 18 will secure a detailed landscaping scheme which should be developed in parallel to environmental improvement projects, in order to establish a coherent approach to the town which also differentiates between different character and functions.

*Views of Bush Walk and the town hall across the square*

55. The proposed development – SU010/APT014 in particular - would partly obscure views across the square towards the rear of the former stable range of the Bush Hotel, now forming Bush Walk, and of the town hall beyond. The applicants acknowledge that the development would obscure views from some positions although the Town Hall would remain visible from the northern part of the square. They explain that the proposed design reflects the need to screen the rear of Superdrug, create a viable depth to the retail unit and residential above and continue a strong frontage to the southern edge of the square. It may be that subtle changes during detailed design could result in a setback to these units which could change the relationship. While the reduction in views would not amount to a reason for refusal it would be regrettable and, therefore, a condition is recommended to review this aspect of the scheme (Condition 12).

*Market Place and Peach Street*

56. 36 Market Place (Superdrug and Costa Coffee) is a two-storey building (9.4

metres to the ridgeline) with two residential flats on the first floor. No changes are proposed at ground-floor level but the first floor flats are to be refurbished including replacement of the existing, unsympathetic windows: the rather small, square windows will be replaced with larger, vertically proportioned windows; more in keeping with the character of the Conservation Area and improving the appearance of the building. There is some uncertainty over the age of these buildings and a condition is recommended requiring the age of the structure to be confirmed before any alterations take place (Conditions 14 & 15 refer).

57. It is proposed to demolish the row of two and three-storey buildings (11.2 metres and 9.2 metres in height respectively) which currently extend from 34 Market Place (Clinton Cards) around to 22 Peach Street (the contribution the existing buildings make to the Conservation Area was considered in paragraphs 43 and 44). A double-fronted terrace is proposed to extend from the refurbished building at No 36 Market Place to the pedestrian link to the new square. The development facing Peach Street would be three-storey (10.6 metres in height; ground-floor retail with duplex flats above) with a regular vertical rhythm reflecting the fine grain of traditional development in the town (but allowing flexibility internally for different sized retail units). (For comparison the existing two-storey buildings are nine metres rising to up to 12 metres for the three-storey elements and the buildings opposite have 11.8 metre ridges and nine metre eaves). To break up the length of the terrace visually and prevent it appearing uncharacteristically repetitive, a variety of elevation treatments are proposed: a three storey rendered section with a flat roof (reflecting many of the Georgian building frontages in the town centre), a brick section with a mansard roof and lower eaves, providing relief to the elevation, and a three storey section with a shallow pitched roof (again paying homage to Georgian forms) marking the corner of the pedestrian access. The detailed elevations, general details drawings and DAS show that the intention is that detailing of the elevations and windows (with projecting cills and deep reveals) will add relief and interest to the elevations. Condition 11 would secure further detail. The development facing onto the new square would be single storey (6.4 metres) on the north side of the square, or two-storey (eight metres) with town centre uses on the ground-floor and flats above on the south side.



Figure 3: Proposed Market Place/Peach Street elevation with existing roofline dotted

58. On the opposite side of the pedestrian access a larger, two-storey retail unit (SU006) is proposed (to meet the need identified in paragraph 32). A key issue here was to achieve the functional requirements of a modern retail building while avoiding a jarring juxtaposition with the relatively small scale of the Redan. A triple gable is proposed – gables are a commonly occurring form in the town centre - which drops down at the sides of the building, minimising the contrast in height with the neighbouring historic building. The new building would be 13 metres to the ridge falling to 8.8 metres at the eaves, while the Redan is eight metres to the ridge. (See also the comments from English Heritage in the summary of consultation responses).

59. The height of the gables and the brick detailing have been criticised. In response the applicants have explained that (to meet the need for modern, flexible retail accommodation and in particular larger units) the unit has been designed to allow trading from both floors. This requires a clear height of 3.5 metres on each floor. The steep roof pitch (52°) is the same as the Methodist Church and a shallower pitch would result in a more 'dumpy', industrial looking appearance at odds with the vernacular.
60. Use of brickwork detailing to break the expanse of brickwork and add interest to elevations is characteristic of the town and is an appropriate response. The detailing – which has been shown indicatively at this stage - will be critical to the success of the building and a condition requiring approval of this detail is recommended (Conditions 8 & 13).

### Rose Street

61. 1-5 Rose Street is currently a three-storey, flat-roofed building with commercial uses (Strange Jewellers and Boots Opticians) on the ground and first floors and two residential flats on the second-floor. This building is to be retained and refurbished. The strong horizontal emphasis of the building and very small, square first-floor windows are unsympathetic to the character of the area and it is proposed to re-elevate it to introduce a more vertical rhythm and finer grain, more typical of the town. The windows would be replaced with larger, rectangular openings the proportions of which would be more in keeping with the character of the area as well as providing better natural light.
62. The existing, rather confined pedestrian access at the end of this building would be opened up to provide a more formal and attractive entrance to the new square in the interior of the block. This would provide controlled access for servicing but, for the majority of the time would be pedestrian only.

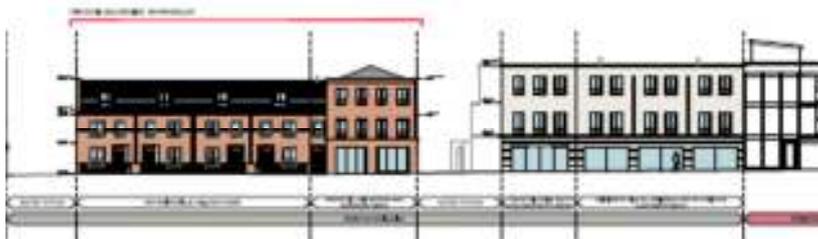


Figure 4: Proposed Rose Street elevations

63. To the north-east of this a new building would be constructed to fill the existing, uncharacteristic gap between Nos 1-5 and the Methodist Church. On the corner would be a retail unit with two flats above (8.8 metres to the parapet and 10.2 metres to the pinnacle of the roof). The retail frontage would wrap around the corner into the square while the Rose Street frontage would be continued with a terrace of four houses. These would provide an effective transition – in scale and character - from the larger scale, 20<sup>th</sup> century commercial buildings on the corner of Market Place and Rose Street to the more domestic, vernacular buildings further along Rose Street. The houses would be nine metres to the ridge and 6.2 metres to the parapet, roughly 1.2 metres higher than the building immediately opposite (No 10), which is 7.8 metre ridge and 4.8 metres to the eaves. They

would be modern homes but their design would reflect the traditional character of domestic buildings nearby and their scale, proportions, materials and detailing are appropriate. The flat-roof, metal clad dormers have been criticised in representations but this is a traditional form, found elsewhere in Rose Street and can provide a less bulky than a pitched roof dormer. Subject to their detailing being agreed (Condition 13) the dormers are considered appropriate.

#### Location of plant and other equipment

64. Two Zones for location of plant are identified on the roof plan Drawing No BNY-PD-08-GP-3001 Rev A10: one at first-floor level at the rear of unit SU010 where it will be screened by first-floor flats; the other at second-floor level over SU007, which will be set back from the edge of the roof to prevent it being unduly prominent. The applicants indicate that the units have been designed to accommodate all A Class uses, including mechanical and natural ventilation systems. Condition 34 requires approval for installation of plant other than in these identified locations and approval of ducting routes.

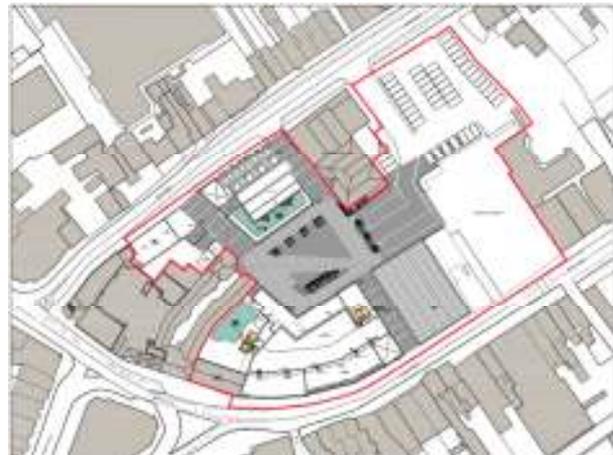


Figure 5: plant and PV zones

#### Making efficient use of land and the density of development

65. Core Strategy Policy CP3 and CP5 require efficient use of land which makes use of the full potential for complimentary uses and the Borough Design Guide establishes that assessment of an appropriate density for residential development must be design-led.
66. Consistent with MDDL Policy SAL08, the proposal is for a mixed use development, primarily for town centre uses with residential development on the upper floors and beyond retail frontages. In these circumstances, achieving an appropriate mix of uses and high quality design are the key considerations. While the residential density is not particularly high, the proposal fulfils the objectives of using the potential for complimentary uses and making efficient use of land by virtue of being a mixed use development. By providing a variety of uses it will help fulfil the objectives of the Masterplan, in particular a thriving high street and an eighteen hour economy (paragraph 23).

#### Dwelling Mix

67. Core Strategy Policy CP5 and MDDL Policy TB05 seek to provide a mix and balance of dwelling types and sizes, which have regard to both the underlying character of the area and the current and projected needs of households.
68. The proposals would provide predominantly two-bedroom flats, which is to be expected in a mixed use, town centre development but adds to the variety of stock in that the accommodation is relatively spacious compared to many town centre flats (see paragraph 78) and provides private, external amenity space which is

relatively unusual in the heart of the town centre. It also offers some variety in the form of the four, three-bedroom houses which help the development fit in with the more domestic character of Rose Street. Thus, the scheme successfully achieves a variety of dwelling types and sizes in a way that is appropriate to the character of the area.

#### Residential amenity of the proposed dwellings

69. In addition to the overarching requirement for good design, Core Strategy Policy CP3 requires that development should not harm the amenity of adjacent sites.

#### Separation distances between dwellings

70. To maintain acceptable levels of privacy, the Borough Design Guide establishes a separation distance of 10 metres front-to-front across the street, whilst acknowledging that schemes in more urban settings or with a more intimate character may require a tighter, more compact layout.
71. The separation distances between the proposed new dwellings and neighbouring properties on Rose Street would be 16 metres. The separation distances across Peach Street, would be 11 metres, slightly more than is currently the case because the upper floors of the proposed development would not project over the pavement as the existing building does.
72. The separation distance between the first and second-floor flats APT 010, 011 and 015 and the two-storey retail unit SU006 would be six metres, across the pedestrian link between Peach Street and the new square (1-2.5 metres wider than the existing Arcade). However, the expectation of privacy is not as great in a town centre location and – balancing a number of considerations – the relationship is considered acceptable. The living rooms of these flats and one of the bedrooms in APT 10 would have dual aspect, either into the new square or over Peach Street, providing more open views and preventing them feeling excessively enclosed. It would only be bedrooms that would face solely towards the retail unit and generally these rooms would not be occupied when the retail unit was open. Thus, acceptable privacy for occupants could be achieved. Narrow walkways leading off from the main streets are a characteristic feature of Wokingham and a wider path would be less successful in character terms. Furthermore, the natural surveillance provided by the flats would improve the safety of the pedestrian link.
73. There would in effect be a front-to-front relationship between the first-floor flats around the new square and the duplex flats along the Peach Street frontage, as each flat would have its own entrance via the central roof terrace. The horizontal separation would be a minimum of 12 metres between elevations and although there would be scope for some overlooking of the first-floor flats from the second-floor terraces, the extent of overlooking from within the buildings would be reduced by the difference in levels. How these private terraces are enclosed will be critical to their success and a condition requiring submission of details is recommended (Condition 18).
74. The proposed development would not have an adverse impact upon neighbouring properties and would also achieve appropriate amenity for future occupants.

### Internal space standards

75. The Borough Design Guide and MDDL Policy TB07 establish minimum internal space standards for new dwellings: the overall floor area (Gross Internal Area, GIA) depends on the number of bedrooms proposed and the habitable area (the combined living, dining and kitchen space) depends on the number of occupants the dwelling is designed to accommodate. The table below shows that the proposed dwellings - both new build and the four refurbished flats - all comply with the minimum standards and in many cases comfortably exceed them.

Dwelling size		Proposed	Recommended (MDDL TB07)	Standard met? ✓ ✗
1-bedroom/ 2-person flat	Floor area (GIA)	56m <sup>2</sup>	50m <sup>2</sup>	✓
	Habitable area	29m <sup>2</sup>	23m <sup>2</sup>	✓
2-bedroom/ 3-person flat	Floor area (GIA)	62-92m <sup>2</sup>	61m <sup>2</sup>	✓
	Habitable area	25-33m <sup>2</sup>	25m <sup>2</sup>	✓
2-bedroom/ 4-person flat	Floor area (GIA)	74-129m <sup>2</sup>	61m <sup>2</sup>	✓
	Habitable area	33-40m <sup>2</sup>	27m <sup>2</sup>	✓
3- bedroom/ 6-person house	Floor area (GIA)	100m <sup>2</sup>	87m <sup>2</sup>	✓
	Habitable area	32m <sup>2</sup>	31m <sup>2</sup>	✓

### External amenity space

76. The Borough Design Guide establishes that dwellings should have access to some form of amenity space, preferably in the form of a private or communal garden.
77. For houses this should generally be a garden although, in areas with an urban character such as Wokingham Town Centre, more compact gardens or alternative private amenity spaces may be appropriate to create a more urban development form that relates to the local character. Consistent with this advice, the four proposed houses would each have a small courtyard garden.
78. While occupants of upper-floor flats rarely have access to a garden, they should be provided with private outdoor space in the form of a balcony, roof terrace or winter garden. Consistent with this advice, 16 of the 18 new flats would have a private terrace. Condition 18 will secure appropriate enclosure.
79. The two existing flats at 1-5 Rose Street currently have balconies, which overhang the pavement below. As part of the proposal these external spaces would be demolished. The balconies are north facing and offer limited amenity: in this case the benefit in terms of the appearance of the building outweighs the harm.
80. Occupants of the new development would also have easy access to a variety of public open space in the town centre: the new square within the development, the Market Place, Howard Palmer Park and Elms Field.

### Environmental Health Issues

81. Core Strategy Policy CP1 establishes that development should avoid areas where pollution (including noise) could impact upon the amenity of future occupants. The proposal is for a mixed use development and consideration should be given both to the residential amenity of future occupants and also the impact that proposed town centre uses may have on residents within the development and nearby. The following paragraphs consider air quality (paragraphs 82-8483), odour (paragraph 85**Error! Reference source not found.**), the impact of noise on residential amenity of the proposed dwellings (paragraphs 86-88), noise generated by the proposed uses (paragraphs 89-91), construction noise (paragraphs 93-**Error! Reference source not found.**) and contamination (paragraph 94).

#### Air Quality

82. A recent air quality assessment, carried out on behalf of the Council, has shown that there are breaches of the air quality objective for nitrogen dioxide (NO<sub>2</sub>) from traffic emissions in the vicinity of the application site. Consequently the area is likely to be declared as an Air Quality management Area (AQMA). Although the AQMA has not yet been declared, there is a known air quality issue and, therefore, consideration should be given to the impact on future occupants and whether the proposed development could exacerbate the issue.
83. The application was accompanied by a report which demonstrates that there will be no exceedance of the air quality objective for nitrogen dioxide in the vicinity of the development in the longer term: the report also concludes that changes to traffic flows in the vicinity will have a negligible impact. Hence, there is no reason to restrict development on air quality grounds.
84. The report also considers the impact of dust and emissions during demolition and construction on air quality. It demonstrates that with good site practice - as set out in the outline Construction Environmental Management Plan (CEMP) that accompanies the application - the construction works would have a negligible effect. A condition is recommended to secure a more detailed CEMP once the main contractor is appointed (Condition 28).

#### Odour

85. The proposed uses include Class A3 restaurants and cafés and Class A5 hot food takeaways. Food outlets can cause nuisance due to cooking smells if not adequately ventilated: in this case there is potential for future occupants of the proposed development to be affected by existing sources and also for new outlets within the development to affect neighbours both within the site and in the wider area. There are no existing problems of such magnitude as to prevent new residential development taking place within the town centre and, subject to a condition to secure implementation of a “ventilation strategy” (Condition 34), there should be no harm to amenity due to odours from new food outlets within the development.

#### Noise: the impact upon the residential amenity of future occupants

86. Core Strategy Policy CP1 seeks to avoid development in areas where noise may impact on the amenity of future occupants and MDDL Policy CC06 reinforces this, requiring proposals to demonstrate how noise impacts on sensitive receptors (both existing and proposed) have been addressed.

87. The A329 Peach Street is one of the main routes through the town centre and consequently road traffic noise in the vicinity of the site is reasonably high. The application is supported by a noise assessment which demonstrates that appropriate internal noise levels (at No Observed Effect Level) can be achieved in all of the proposed residential properties by the use of closed double-glazing with alternative ventilation. While reliance on such measures is not generally encouraged, future occupants will balance this inconvenience against the significant benefits of living in a town centre location. In the wider public interest, town centre dwellings will add to its vitality and help to achieve the policy objective of a thriving town centre. Hence, it is an appropriate compromise in this situation. Details of the specific systems to be used should be agreed at the detailed design stage (Condition 32).
88. The terraced houses would have small gardens and the majority of the flats would have a private terrace. These would be located around the square, where the mass of the surrounding buildings would screen them from road noise to a significant extent. The range of noise levels predicted on the terraces would extend into the lower end of Lowest Observed Adverse Effect Level; the level above which adverse effects on health and quality of life can be detected. However, Annex 1 of the MDDLDP accepts that in some circumstances it is preferable to have a private, external amenity space (with higher noise levels) than none at all: this is such a case.

Noise: the impact of the proposed development

89. In addition to the requirements of Core Strategy Policy CP1 and MDDLDP Policy CC06, MDDLDP Policy TB20 establishes that the servicing arrangements for retail development should not harm residential amenity due to noise, fumes or disturbance.
90. The introduction of 21 additional dwellings in this town centre location would be insignificant in terms of levels of activity and potential disturbance of neighbouring properties.
91. The application seeks a flexible consent that would permit use of the commercial premises as cafes or restaurants (Class A3), pubs (Class A4) and takeaways (Class A5). While a range of town centre uses is necessary in order to achieve the aim of a thriving town centre, they should be managed in such a way as to remain compatible with other, more sensitive town centre uses, in particular dwellings. Use of the premises would also be controlled by condition. Use of sound amplifying equipment would be controlled under a premises licence. Conditions 30 & 31 **Error! Reference source not found.** refer.
92. The applicants propose that all deliveries to retail and restaurant premises will be between 07:00-21:00 and that this will be secured by lease. Consideration will also need to be given to avoiding conflict when the square is busy and a condition securing a strategy for managing deliveries between these times is recommended (Condition 23).

Construction Noise

93. Noise from construction activity can be minimised using Best Practicable Means

(BPM) techniques. An Outline CEMP Report (December 2014) accompanied the application, to be amplified once a contractor is in place. The proposed condition (Condition 29) is based on the condition for Phase 1 of the Regeneration of the Peach Place site.

#### Contamination

94. As with many sites in the town centre, the site is identified as potentially being contaminated from historic uses. Hence, the application was accompanied by a Phase 1 Geo-Environmental Assessment (December 2014). The report does not identify contamination of such magnitude as to be a significant constraint on development but further assessment is required and a condition to secure this, together with remediation and validation if necessary (Condition 35).

#### Access and Movement

95. Core Strategy Policies CP1 and CP6 require proposals to reduce the need to travel, particularly by private car, and consideration of the traffic impacts of development.

#### Accessibility of the site

96. Core Strategy Policy CP6 also directs development to locations where there are or will be choices of mode of transport and minimise the distance people need to travel. The site is accessibly located in the town centre, where there is a choice of modes of travel available (for occupants and visitors alike) and excellent access to facilities.
97. The policy also requires improvements to existing transport infrastructure including road, rail, public transport and facilities for pedestrians and cyclists, including those with reduced mobility.
98. Policy CP10 identifies a series of improvements to the strategic transport network, several of which will support the regeneration of the town centre by improving access by sustainable modes and reducing congestion. These include the station link road and associated improvements to the public transport interchange at Wokingham Station, which are already in progress, and measures to support cycling and walking. Public realm improvements to make it easier to travel into and circulate around the town on foot any by bicycle will be critical to the overall success of the town centre. Hence, a significant proportion of the funding available to mitigate the impact of the development is proposed to go towards environmental improvements in the town centre (see paragraph 158).

#### Traffic Generation

99. The applicants' transport statement demonstrates that the net gain of 1,221m<sup>2</sup> commercial floor space and 21 dwellings will not generate significant additional movements or give rise to any significant highway issues. Indeed the loss of the Rose Street Car Park will lead to a localised reduction in the number of movements in Rose Street.

#### Construction Traffic

While the amount of construction traffic will not be significant, there will be peaks in activity which will need to be managed through a Construction Environmental

Management Plan (CEMP). Condition 28 refers.

### Pedestrian circulation

100. As well as Bush Walk there would be two pedestrian links into the square from Rose Street and one from Peach Street. This would maintain pedestrian connectivity, providing alternative routes away from the more heavily trafficked roads. The new square would provide a considerably improved pedestrian environment compared to the existing car park, which is currently framed by the backs of buildings. The square is proposed to be level, with level access at the thresholds of the shops.
101. The application does not include any proposals to improve either the streets immediately surrounding the site or links to points of arrival in the town, in particular along Peach Street to the Easthampstead Road car park. However, it will contribute financially towards environmental improvements in the town centre (see paragraph 158) and the scope for improvements in Peach Street is one of the areas which is due to be considered. (Planning permission F/2014/1317, which was approved on 2 February 2015, secures land through a S106 agreement to facilitate the realignment of the Peach Street carriageway and widening of the footpaths in the vicinity of the Overhangs on the route from Easthampstead Road car park).

### Servicing

102. MDDL Policy TB20 requires retail proposals to make provision for servicing without harm to amenity, highway safety visual amenity or any other adverse environmental impact.
103. Delivery vehicles can currently approach the site either from the east, via Cross Street, or the west, via Broad Street. Due to width restrictions to the west on Rose Street, large vehicles leaving the site have to travel eastwards and exit via Wiltshire Road. The proposals would not change this.
104. The existing commercial development on the site is serviced primarily from the Rose Street car park. Under the current proposals this space would become a public square but would also continue to be used for servicing and deliveries. Access and egress would be from two separate points on Rose Street, allowing traffic to enter and leave the site in a forward direction. Tracking drawings have been provided which show that the largest vehicles that would be likely to need to access the site - an 11.5 metre long rigid refuse collection vehicle or a 12 metre long rigid delivery truck - would be able to make this manoeuvre and a safety audit has been requested. The tracking shows that vehicles turning right out of the site would impact on a short stay parking space in front of the Methodist Church: removal of this space would require an amendment to the TRO. Access to the service yard would be controlled by removable bollards, to avoid conflict between pedestrians and vehicles.
105. In addition to the main servicing route through the square, a small service yard is proposed at the rear of 38-42 Market Place and 1-5 Rose Street where deliveries by smaller vehicles could take place during the daytime under the control of the site management. Vehicles would be able to turn within the site, unload and deliver goods without entering the square itself. This is acceptable subject to the

detail to be provided through the Servicing Management Plan (Condition 23).

106. The applicant anticipates that A1 retail units will require one or two deliveries per week and A3 cafés and restaurants will require a daily delivery. It is proposed that deliveries will take place between the hours of 07:00 and 21:00, with allocated delivery days and times controlled via lease agreements and management company procedures. Condition 23 requires this strategy for servicing management strategy to be refined and implemented.
107. This arrangement would make good use of space (a dedicated service yard would occupy a large proportion of this town centre site but would only be used for a limited time each day) and could be successful, with appropriate management, to ensure that deliveries and associated activity do not take place at unsociable hours for neighbouring residents, or else when the square would be busy.
108. Servicing arrangements for the M&S store would remain unaltered.
109. The proposals would not prejudice future delivery of the service road, safeguarded under MDDL Policies CC08, TB20 and Appendix 3: the service road is proposed to extend from Cross Street to the boundary of 38 Peach Street (the M&S car park) and would facilitate rear servicing of properties on the northern side of the street.

#### Refuse storage

110. Core Strategy Policy CP1 and MDDL Policy CC04 require development to make provision for suitable waste management facilities including provision for recycling.
111. Separate, appropriately sized stores for domestic and commercial refuse are proposed, together with access for refuse vehicles (see paragraph 104): Condition 24 would secure this provision.

#### Car Parking for staff and visiting members of the public

112. Core Strategy Policies CP6 and CP14 4) h) require development to provide appropriate vehicular parking, to facilitate a viable and sustainable town centre and MDDL Policy CC07 together with Appendix 2 establishes parking standards for different types of development. However, the parking requirements for the application cannot be reviewed in isolation but must be considered in their wider, town centre context.
113. A number of the sites identified in the Masterplan SDP for development, including part of the Peach Place site, are currently car parks and appropriate parking provision - to offset these losses and to meet increased demand - is crucial to the successful rejuvenation of the town. The loss of short-stay parking, including three disabled parking bays, in the existing Rose Street car park is the key concern of the majority of those objecting to the application.
114. The Masterplan SDP establishes the approach towards provision of parking for the town centre, whereby medium and long stay parking is provided at a series of gateway car parks around the town centre, to reduce the need to travel by car into the heart of the town centre, assist in reducing congestion and improving the environment for people (paragraphs 7.4.1-7.4.6 and Figure 35). Short stay

parking would continue to be provided in central locations and, indeed, a key objective for the area north of the Market Place is to continue to provide convenient short stay car parking for town centre visitors (paragraph 12.3.2) while paragraph 12.3.10 states that *“the number of spaces must be retained, although different configurations are possible”*. Paragraph 12.3.23 establishes the aim of providing a high quality public space within the Peach Place development and, while the following paragraph establishes that car parking provision may be integrated into the design (where it does not undermine the objectives for the creation of a pedestrian only area), it is apparent from Figures 34, 35 and 39 that the expectation was that the existing short-stay parking would not remain in its current location.

115. The SPD acknowledges that the strategy will need to be developed in more detail, alongside parking demand management and measures to encourage travel by alternative, sustainable means. There is an emerging Wokingham Town Centre Car Park Management Plan which is due to be reported to Executive within the next few months. The document will include a review of existing parking provision and capacity (which has already taken place) and will establish how much parking will be required and where it should be located in the longer term. One proposed element of the Car Park Management Plan will be a new multi-storey car park, to be delivered in conjunction with a leisure hub, at the Carnival Pool site. Consultation on these proposals is currently taking place (9 February-6 March 2015).
116. Due to existing tenancies, the application scheme – if approved – would be unlikely to start on site until late 2016 at the earliest, by which time the parking strategy will have been resolved. Nevertheless, as the application is being assessed in advance of the Car Park Management Plan, it needs to be demonstrated that there will be sufficient capacity even without the Car Park Management Plan.
117. The applicants propose to relocate the three disabled parking bays to Rose Street, immediately in front of the Methodist Church, where there are currently five short-stay parking bays. This would require a Traffic Regulation Order, which would be the subject of a separate regulatory system. Condition 26 would secure provision of replacement disabled parking bays either in this location or elsewhere in the town centre.
118. MDDL Policy CC07 together with Appendix 2 establishes parking standards: one space/20m<sup>2</sup> for A1(non-food) retail uses and A2 Financial and professional services uses, increasing to one space/14m<sup>2</sup> over 1,000m<sup>2</sup>. The requirement for A3 restaurants and cafes is one space /5m<sup>2</sup> and one space/3m<sup>2</sup> for drinking establishments and takeaways.
119. The application seeks a flexible consent for A Class use but is expected to be retail led. Condition 3 has been recommended primarily to protect the retail character of the primary shopping frontage but will also serve to limit the floor area of non-retail uses within the development as a whole. The likelihood is that daytime visitors to cafes, restaurants and pubs will also be shoppers; during the evening when the number of visitors to restaurants and pubs is likely to be higher, demand from shoppers and people working in the town centre will be lower. Hence, it is considered reasonable to apply the standards for A1 use.

120. The proposal will result in a net gain of 1,221m<sup>2</sup>, generating a requirement for 66 additional spaces. In addition it will be necessary to ensure there is capacity for the 99 spaces that would be dispersed by the development, bringing the total requirement to 165 spaces. The background work already done to support the Car Park Management Plan demonstrates that there is sufficient capacity within existing car parks to accommodate this, in particular Easthampstead Road, which lies within the designated Town Centre and adjacent to the Primary Shopping Area.
121. M&S car park would be reconfigured to make more efficient use of the space and facilitate the public realm improvements required to create the new square but the number of parking spaces would be maintained at 58.

#### Car parking for residents

122. Policy CP6 requires development to provide appropriate vehicular parking, having regard to car ownership. MDDL Policy CC07 together with Appendix 2 establishes a methodology for calculating the parking demand generated by residential development. However, parking standards are only as a starting point and it is recognised that each development site will need to be assessed on its own merits.
123. For a development of 26 dwellings in an urban location, based on the proposed mix of dwellings sizes, all of the dwellings being privately owned and no allocated parking, the Council's parking demand calculator indicates demand for 26 unallocated car parking spaces. However, there are five existing dwellings, which do not have dedicated parking provision, and if these are discounted demand is reduced to 21 spaces. (NB although these figures correspond to the number of dwellings, the calculation is not actually so simple as one space per dwelling).
124. No residential parking is proposed within the scheme. However, the site is located in a highly sustainable, town centre location and occupants would have good access to town centre facilities and public transport, so need not be reliant on vehicular transport. In these circumstances the absence of parking within the scheme is acceptable, particularly as there is capacity in nearby town centre car parks (see paragraph 120 above). Purchasers would make an informed decision balancing the benefits of town centre living against the lack of dedicated parking; they would choose whether to own a car and would have the option of applying for a season ticket for a public car park if desired.

#### Cycle Storage

125. The policies referred to in paragraph 122 also require enhanced facilities for cyclists and establish standards for cycle storage. There is no existing on-site provision.

#### Cycle storage for the commercial premises

126. For A class uses (other than bulky goods warehouses) the requirement is storage for one cycle per 125m<sup>2</sup>, 20% of which should be long-term storage (for staff) and the remaining 80% short-term (for visiting members of the public). The net increase of 1,221m<sup>2</sup> floor space would generate a requirement for 10 cycle

spaces. However, having regard to the lack of existing cycle parking, a more generous provision has been proposed based on the new Class A floor space of 3,374m<sup>2</sup>, which generates a requirement for a total of 29 cycle spaces: six long-term and 23 short-term. A secure store for seven cycles is proposed within the service area to the rear of unit SU012 and Sheffield stands for 30 bicycles are proposed within the square. Condition 27 would secure this provision.

#### Cycle storage for the dwellings

127. Minimum residential cycle storage requirements depend on dwelling type and size (MDDL P Appendix 2): the majority of the flats would have three or fewer habitable rooms and require secure, covered storage for one bicycle; Apartment 010 would have four habitable rooms and the four houses would have five habitable rooms each generating a requirement for storage for two bicycles i.e. 31 in total. However, given the sustainable location and lack of allocated car parking, it is important that measures to support sustainable travel are incorporated in the scheme and it would be desirable to exceed the minimum requirement. Furthermore, the applicant's stated intention is to seek two Code for Sustainable Homes (CfSH) credits for cycle storage, which would require one space for the one-bedroom flat and two each for all the other units i.e. 51 in total.
128. It is proposed to provide storage for two bicycles for each dwelling, including the refurbished flats which do not currently have any cycle storage i.e. 52 in total. For the four houses and flats 001-014 on Peach Street this would be in a private store within their garden or private terrace. The five flats on 1-5 Rose Street would share a store in the service yard at the rear of 1-5 Rose Street. Condition 27 will secure this provision.
129. Whilst, it is generally preferable for cycle storage to be at ground level, individual stores close to the dwelling offer better security and the lift has been specified to accommodate this use.

#### Archaeology

130. Core Strategy Policy CP3 establishes that development should not have a detrimental impact upon heritage assets. This is amplified by MDDL P Policy TB25 which requires developments in areas of high archaeological potential – which include the centre of Wokingham – to provide an assessment of the impact of the development upon archaeological remains and to secure preservation in situ or - where this is not practical - excavation, recording and archiving of remains.
131. The Cultural Heritage Baseline Assessment which accompanies the application provides desk based assessment of the archaeological and historical significance of the site and the potential impact of development upon it.
132. The site is located within the core of the medieval planned town of Wokingham, which dates from the thirteenth century, although it was developed on the site of an earlier settlement. Post-War redevelopment has resulted in the loss of medieval and post-medieval street frontages but there is potential for archaeological remains of regional importance to survive within the site. In particular disturbance in the Rose Street car park is probably limited to a depth of c.0.60m. The report concludes that the site has a medium to high potential for encountering medieval and post-medieval remains of moderate significance.

The desk study alone does not provide sufficient information to fully understand the presence, quality and significance of archaeological remains at the site and a Written Scheme of Investigation (WSI) has also been submitted, which sets out acceptable proposals for a field evaluation the site. This will provide a better understanding of the archaeological implications of the proposed development and inform decisions about conservation of archaeological remains. The Council's Archaeological Advisor recommended that a limited field evaluation should be undertaken prior to determination as part of a phased programme but, failing that the works could be secured by condition. The applicants raised concerns about the disruption that would be caused by carrying out these works while the site is still operational and, therefore, it is recommended that the evaluation is secured by Condition (Condition 36).

### Ecology

133. Core Strategy Policies CP3 and CP7 and MDDL Policy TB23 establish that development should not have a detrimental impact upon important ecological features and any ecological impacts should be mitigated. Furthermore, development proposals should enhance or provide new biodiversity features: the applicant has indicated the intention to seek credits for ecological enhancements in their CfSH and BREEAM pre-assessments.
134. In this case there is an opportunity to incorporate swift nesting boxes in the scheme, in lieu of a contribution towards off-site mitigation. Subject to conditions to secure the nesting boxes and implementation of the measures outlined in the bat report (Condition 38) there are no ecological objections to the proposed development.

### Thames Basin Heaths Special Protection Area

135. The Thames Basin Heaths Special Protection Area (SPA) was classified on 9 March 2005 under the European Birds Directive due to its importance for heathland bird species. Accordingly, under regulation 61 of the Conservation of Habitats and Species Regulations 2010, the Local Planning Authority must decide whether the project, alone or in combination with others, would be likely to have a significant effect upon the Special Protection Area.
136. Core Strategy Policy CP8 and paragraph 4.45, build on the Thames Basin Heaths Delivery Framework, establishing that new residential developments which result in a net gain of one or more dwellings within 5km of the SPA can have a significant impact upon its integrity. Developments of 50 dwellings, within 7km of the SPA can also have a significant effect.
137. The 5km boundary cuts through the application site: the 15 new flats adjacent to Peach Street fall within 5km as does the one flat that is to be lost. Therefore, the net gain within 5k of the SPA is 14 two-bedroom dwellings, the impact of which is proposed to be mitigated by a £35,590.80 (ring-fenced) contribution: £6,896.54 for Strategic Access Monitoring and Management (SAMM) and £28,694.26 towards provision of Suitable Alternative Natural Greenspace (SANG) at the Council operated SANG at Rooks Nest Wood (allocated under MDDL SAL05, see paragraph 158). The net gain on the remainder of the site - seven dwellings - would be below the 50 dwelling threshold so would not be deemed to have a

significant impact.

#### Flooding and Drainage Issues

138. Consistent with the NPPF, Core Strategy Policy CP1 and MDDL Policy CC09 require a sequential approach which directs development away from the areas at highest risk of flooding (from any source). Furthermore, development should avoid increasing and where possible reduce flood risk on the site and elsewhere.
139. Core Strategy Policy CP9 identifies Wokingham as a Major Development Location, capable of accommodating major new development, and MDDL Policy SAL08 allocates the site for mixed use development. Both Local Plan Documents were subject to a Strategic Flood Risk Assessment (FRA) and the site is located in flood zone 1, where the risk of flooding is low and all forms of development - including 'more vulnerable' residential uses and 'less vulnerable' town centre uses - are acceptable. Therefore, the sequential test does not need to be applied.
140. The Environment Agency pluvial flooding maps identify that there are small areas at the rear of the existing buildings which currently are at low risk of surface water flooding.
141. MDDL Policy CC10 requires surface water to be managed in a sustainable manner. For brownfield sites runoff rates should be reduced to as near to greenfield rates as possible. Sustainable Urban Drainage Systems (SUDS) should also be provided where practicable, with appropriate arrangements for adoption by the Council as the Lead Local Flood Authority). There should be no adverse impact on the public sewerage network when discharging to a public sewer.
142. The site area is over one hectare and, therefore, a FRA is required. The site is already largely hard surfaced with only small areas of amenity planting and the proposal would not increase the area of impermeable surfaces. It is drained by a combined system of drainage and foul sewer.
143. An attenuation tank is proposed beneath the central square and there may also be scope to incorporate SUDS solutions such as tree pits, rain gardens, water butts and tanked permeable paving into the design. The proposed surface water drainage systems would prevent flooding during a 1 in 30 year rainfall event and in addition, any flooding that occurred during a rainfall event up to and including 1 in 100 years (plus a 30% allowance for climate change) would be retained within the site.
144. Space constraints mean it would not be possible to accommodate a large enough tank on site for a 1 in 100 year event but it would be possible to restrict the existing run-off rate by 50%. This is an improvement on the existing situation and is acceptable subject to a condition requiring further details of the proposed drainage scheme to be approved and implemented (Condition 41).

#### Sustainable Design and Construction

145. Core Strategy Policy CP1 requires new development to contribute towards the goal of achieving zero carbon development by including on-site renewable energy features and minimising energy and water consumption. This is amplified by

MDDL Policies CC04 and CCP05 as well as the Council's Sustainable Design and Construction Supplementary Planning Document and accompanying Developers' Guide.

146. Policy CC04 establishes that new residential development should seek to achieve at least Code for Sustainable Homes Level 4 and requires all dwellings to meet the internal potable water consumption target of 105 litres or less per person per day. Non-residential developments of over 1,000m<sup>2</sup> should achieve the mandatory Building Research Establishment Assessment Method (BREEAM) requirement and meet or exceed statutory requirements for water resource management.
147. The application is supported by a Sustainability Statement and Code for Sustainable Homes (CfSH), BREEAM domestic refurbishment and BREEAM retail pre-assessments.
148. The CfSH pre-assessment indicates that 15 (68%) of the 22 new build dwellings could achieve Code 4. The remaining seven (flats 006-009 on Peach Street and 2a, 2b and 3 on Rose Street) will be able to achieve Code 3 but are unlikely to achieve Code 4 because they may not be able to meet all the requirements for Lifetime Homes, primarily due to access issues. Given the numerous objectives of this scheme this is considered an appropriate compromise. The four refurbished flats and the commercial premises will achieve BREEAM "Very Good".
149. The pre-assessments also indicate that all of the dwellings will achieve the internal potable water consumption target of 105 litres or less per person per day (Condition 40 refers).
150. Policy CC05 requires schemes for more than ten dwellings (gross) or non-residential proposals for more than 1,000m<sup>2</sup> floor space to achieve a 10% reduction in carbon emissions through renewable energy or low carbon technology. The submitted Energy Statement demonstrates that this can be achieved by a combination of energy efficiency and passive design measures plus on-site renewable energy technologies: Air Source Heat Pumps (ASHP) to meet the space heating and air conditioning needs of the commercial units and 232m<sup>2</sup> Photovoltaic panels (PVs) (slightly angled to allow for self-cleaning) to provide electricity for both the commercial and residential properties. PVs can be unobtrusively located, on flat roofs behind a parapet or screened by upper storey buildings (at the rear of units SU010 and above units SU014-SU016), as identified on Drawing No BNY-PD-08-GP-3001 Rev A10. (Condition 41 refers.)

#### Affordable housing

151. Core Strategy Policy CP5 establishes that, subject to viability, developments of at least five dwellings (net) or on sites of at least 0.16 hectares (net) should provide up to 50% affordable housing. The policy also establishes triggers and minimum requirements depending on the location and whether the land was previously developed. In this case the proposal relates to previously developed land in the Major Development Location of Wokingham and the net gain and site area (21 dwellings on a 1.09 hectare site) exceed the 15 dwellings (net)/ 0.5 hectares threshold, so the policy requirement is for 30% affordable housing. This equates to 6.3 dwellings.

152. The Affordable Housing Group (a Member/Officer reference group) took the view that on-site provision would not be feasible in this case due to high rents (including service charges), lack of car parking and the town centre being an unsuitable location for vulnerable residents. Hence, a commuted sum of £637,000 (index-linked) towards off-site provision was sought.
153. The requirement to provide affordable housing is subject to viability and it has been demonstrated (see paragraph 157) that the full package of infrastructure impact mitigation cannot be delivered without prejudicing delivery of the development, which is itself a Development Plan objective. The Sustainable Communities Infrastructure Working Group (a Member/Officer reference Group) considered how the funding available should be prioritised: since the primary aim of the proposal is regeneration of the town centre, the priority is environmental improvements which directly support this aim. Hence, in this case, no affordable housing is to be delivered – either on or off-site.

#### Infrastructure Impact Mitigation

154. Core Strategy policy CP4 requires that infrastructure, services, community and other facilities are improved to meet the requirements of new development, taking into account cumulative impact. Where development gives rise to the need to provide new or improved services, this will be secured by planning obligation or condition as appropriate. These requirements are amplified by Policy CP3 g) and MDDL Policy TB08 in terms of open space provision, Section 13.7 of the Wokingham Town Centre Masterplan SPD and the Council's Planning Advice Note. However, viability is also a consideration and the NPPF emphasises that development should be deliverable.
155. The proposal would result in a net gain of 1,221m<sup>2</sup> commercial floor space and 21 dwellings. Subject to viability, the Local Planning Authority would normally seek to secure a package of a similar value to other strategic sites: around £28k per dwelling plus affordable housing i.e. c£588k based on a net gain of 21 dwellings plus a commuted sum of £637,000 for affordable housing (see paragraph 152 above) bringing the total package to £1.225 million.
156. However, the costs of delivering the redevelopment of a town centre site are considerable and it is unlikely that regeneration of the Peach Place site would be viable under normal circumstances (as demonstrated by the failure of three previous applications for comprehensive redevelopment of the site to come to fruition). It is for this reason that the Council has taken a lead in land assembly and delivering the regeneration of Peach Place.
157. The application was accompanied by a financial appraisal which has been reviewed by an independent financial advisor, acting on behalf of the Local Planning Authority. The appraisal was based on an assumption of the scheme achieving a return of only 5%, rather than the generally accepted developer's profit of around 18-20%. This was a decision taken by the Wokingham Town Centre Development Board in order to bring forward the development, which would not have been deliverable otherwise but will contribute significantly towards achieving the Council's wider aim of rejuvenating the town centre. The Town Centre Development Board and the Town Centre Coordination Board jointly agreed that any return over and above the 5% level would be used to mitigate the impact of

the development.

158. The financial appraisal established that the amount available for infrastructure impact mitigation would be in the vicinity of £325,000 (plus around £36,000 towards mitigation of the impact of new residential development on the Special Protection Area, ring-fenced as failure to mitigate this would be fatal to the application). The Sustainable Communities Infrastructure Working Group (a Member/Officer Reference Group) has considered prioritisation of the available funding as follows (the first three items having been calculated on the basis of the net gain of the net gain of 17 two-bedroom and four three-bedroom dwellings):

<b>Infrastructure</b>	<b>Contribution</b>
<b>Access and Movement (My Journey sustainable travel)</b>	<b>£9,450</b>
Mainstream Primary Education	£82,106
Primary Special Educational Needs	£19,532
Mainstream Secondary Education	£13,188
Secondary Special Educational Needs	£2,084
Post -16	£3,260
<b>Total Education</b>	<b>£120,170</b>
Suitable Alternative Natural Greenspace (SANG)	£6,897
Strategic Access Monitoring and Management (SAMM)	£28,694
<b>Mitigation of the impact upon SPA (ring-fenced contribution )</b>	<b>£35,590</b>
<b>Environmental Improvements in the Town Centre</b>	<b>c £192,790</b>
<b>TOTAL</b>	<b>c £358,000</b>

159. However, there are a number of variables which will not be confirmed until later in the application process which could result in the overall value of the package going either up or down. Due to the level of uncertainty over the final costs, the recommendation is that the approach to calculating the value of the package be agreed at this stage, together with trigger points for review of the package and to make payments. The first review would be three months prior to commencement, at which stage it will be possible to establish the overall value of the package will be with more certainty. The second review would be at practical completion, at which stage it is anticipated the major proportion of the payments would be made. The final review would be two years after practical completion, when the final payment would be made. This approach would assist with the cash flow of a project which would not be viable in normal circumstances. It also maximises the amount available for impact mitigation whilst avoiding the risk of the Council (as developer) failing to meet the 5% return on scheme costs threshold set by the Town Centre Development Board.
160. As the Council is both one of the applicants and the regulatory authority, it is not possible to enter into a S106 legal agreement in the usual way and, as an alternative mechanism for securing the funds, on 14 January 2015 the Executive agreed the principle of the release of monies: an updated scheme viability

appraisal is to be reported back to the Executive in advance of the first payment.

### Economy

161. Using Homes and Communities Agency (HCA) guidance, the applicants have estimated that the proposed development is expected to generate about 107 new jobs, adding approximately £4,639,000 per annum to the local economy (retail employment densities are dependent more on turnover than floor area and are likely to be significantly higher in the new development than is currently the case with older properties, occupied largely by start-up businesses. In addition it is estimated that 44 construction jobs per annum will be generated during the 18-24 month construction period. New residents' expenditure will also add approximately £642,127 per annum to the local economy.
162. MDDL Policy TB12 expects major development proposals to be accompanied by an Employment and Skills plan to show how the proposal accords opportunities for training, apprenticeship or other vocational initiatives to develop local employability skills required by developers, contractors or end users of the proposal. Condition 43 refers.

### **CONCLUSION**

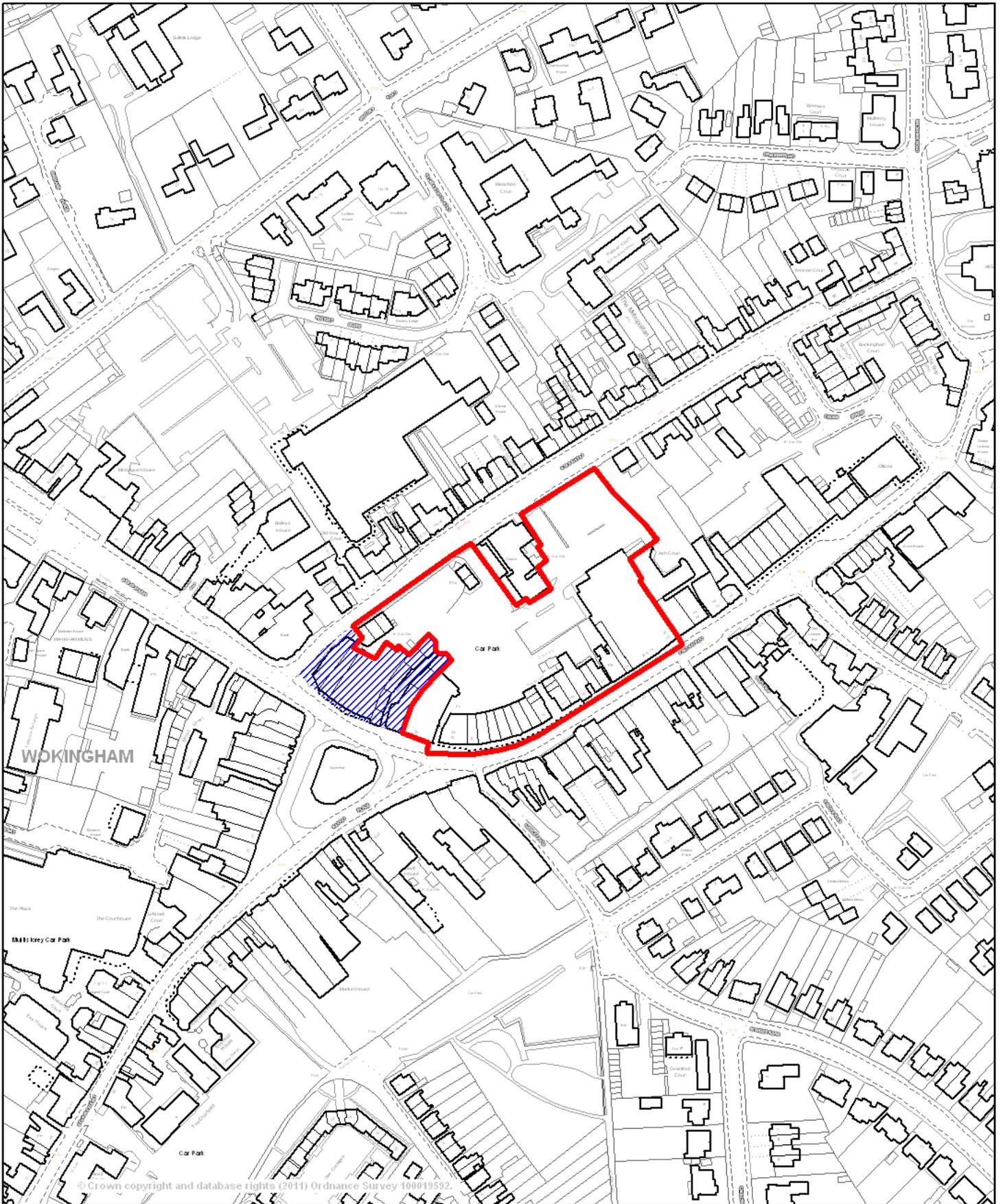
The proposal will deliver the next step in the regeneration of the town centre, replacing or refurbishing existing poor quality buildings and providing a mix of town centre uses, including a new public square, to support a vibrant town centre. It is consistent with Development Plan Policy and the guidance in the Wokingham Town Centre Masterplan SPD, which seek to secure the growth and renaissance of Wokingham the town centre and can be supported.

### **CONTACT DETAILS**

<b>Service</b>	<b>Telephone</b>	<b>Email</b>
Development Management and Regulatory Services	0118 974 6428 / 6429	development.control@wokingham.gov.uk

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# Location of Planning Application -



**WOKINGHAM**  
BOROUGH COUNCIL

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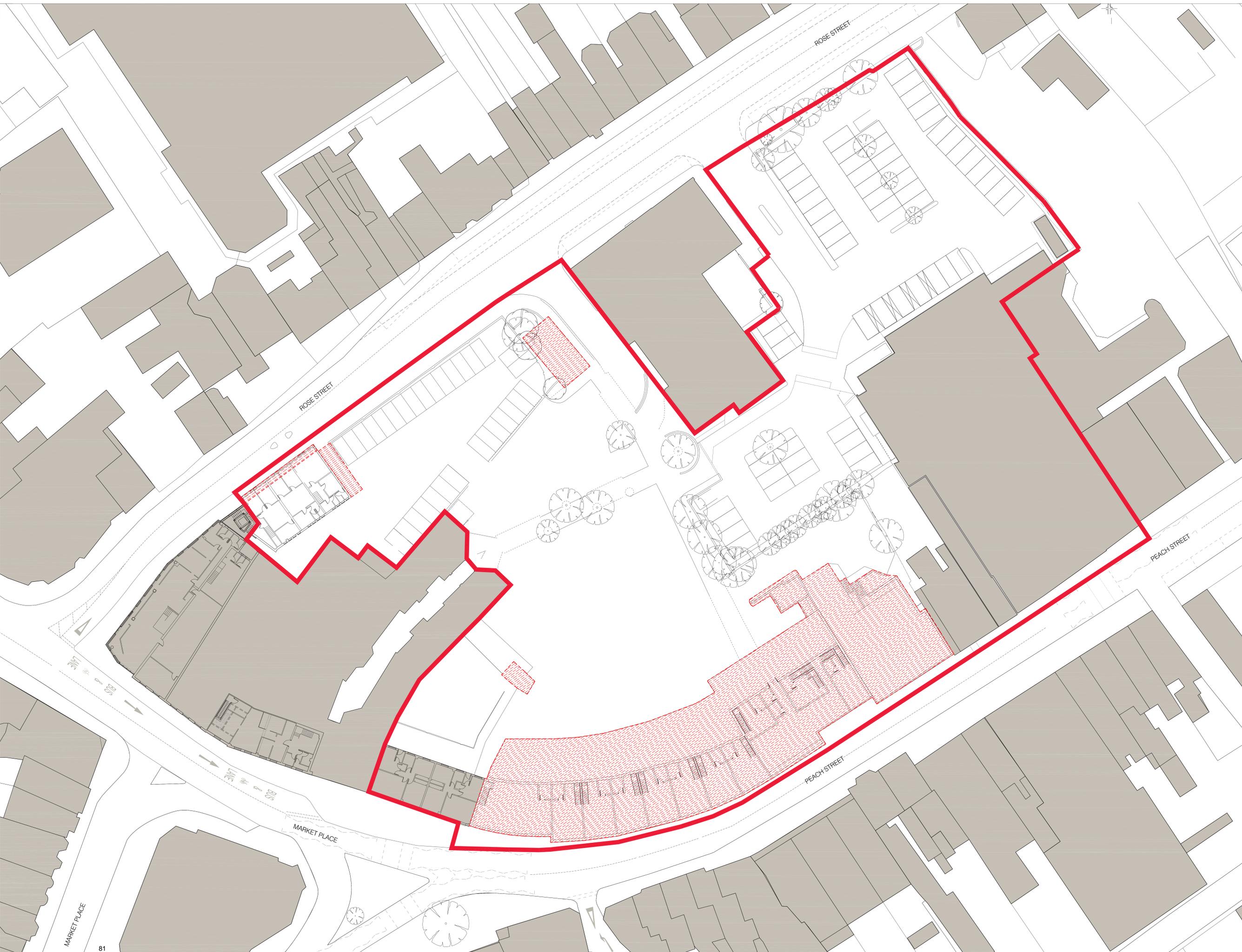


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**APPLICATION BOUNDARY**  
**AREAS TO BE DEMOLISHED**



A04	08.11.14	General Update	DR	EP
A03	17.10.14	New Scheme	DR	EP
A02	16.12.13	Hako Chinese Confirmed in Demolition	EP	EP
A01	03.03.13	General Update	DR	EP
A00	08.08.13	Scheme Updates	TD	EP

Rev | Date | Description | By | Check



Client  
**WILSON BOWDEN DEVELOPMENTS LIMITED**

Project  
**PEACH PLACE DEVELOPMENT WOKINGHAM**

Drawing Title  
**DEMOLITION FIRST FLOOR PLAN**

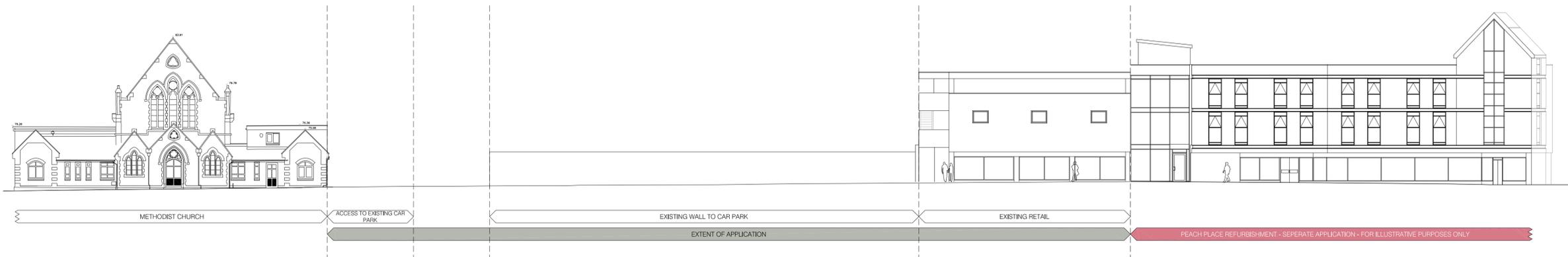
Drawn By: TD | Checked By: EP

Scale: 1:200 @ A0 | Date: AUGUST 2013

Project No: 4141 | Drawing No: BNY-PD-05-1002 | Revision: A04

Status: **PLANNING**  
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ELEVATION A-A\_ ROSE STREET

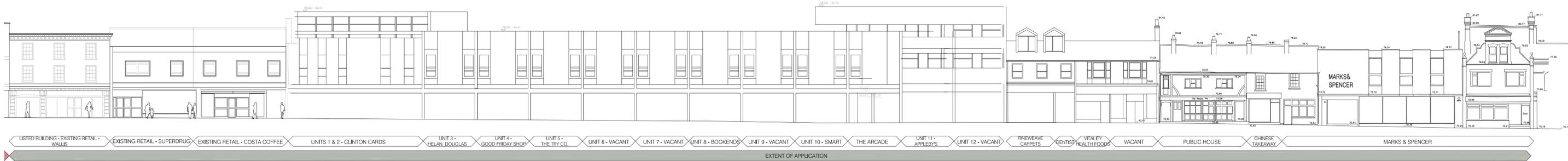
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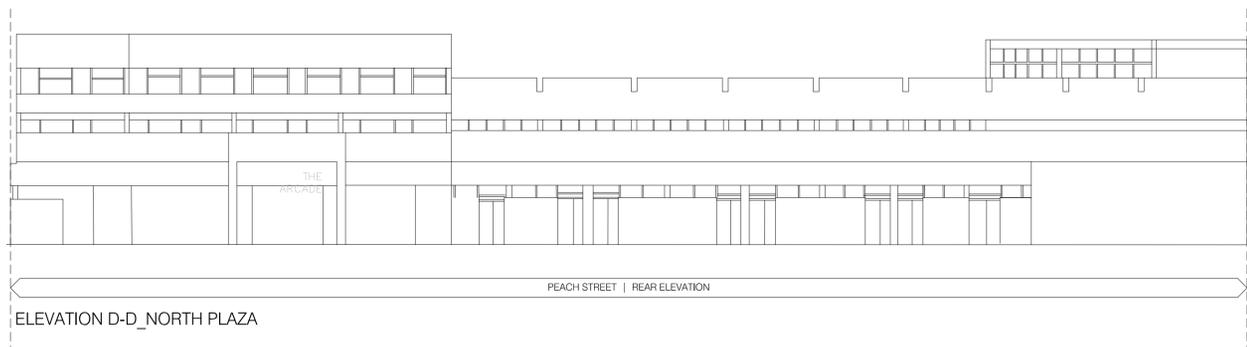
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The CDM hazard management procedures for the Benoy aspects of the design of this project are to be based on the Benoy - Designer's Hazard Identification and Management Sheet and drawings. The full project design team comprehensive set of hazard management procedures are available from the Planning Supervisor/Safety Coordinator appointed for the project.

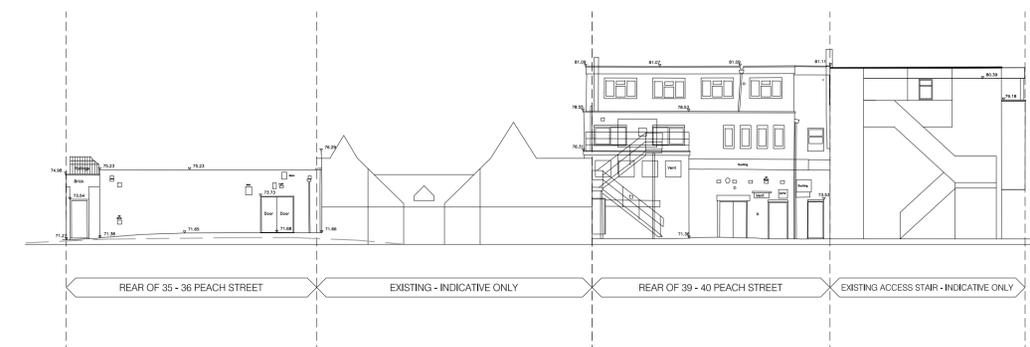


ELEVATION B-B\_ PEACH STREET

ELEVATION C-C\_ PEACH STREET



ELEVATION D-D\_ NORTH PLAZA



ELEVATION E-E\_ EAST PLAZA



ELEVATION F-F\_ SOUTH PLAZA

A07	28.11.14	General Update	DRI	EP
A06	17.11.14	New Scheme	DRI	EP
A05	10.09.13	General Update	DSh	EP
A04	01.07.13	Elevation BB Updated	RM	EP
A03	01.02.13	South Plaza elevation extended	TH	EP
A02	14.01.13	General amendments	TH	EP
A01	05.11.12	Existing M&S Peach Street Elevation Included	TH	EP

Rev	Date	Description	By	Chkd
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**Benoy**  
Design Architecture Masterplanning Interiors Graphics

**Client**  
WILSON BOWDEN DEVELOPMENTS LIMITED

**Project**  
PEACH PLACE DEVELOPMENT WOKINGHAM

**Drawing Title**  
EXISTING ELEVATIONS

**Drawn By**  
TH

**Checked By**  
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**Date**  
FEBRUARY 2013

**Project No.**  
4141

**Drawing No.**  
BNY-PD-08-GE LL01 A07

**Revision**  
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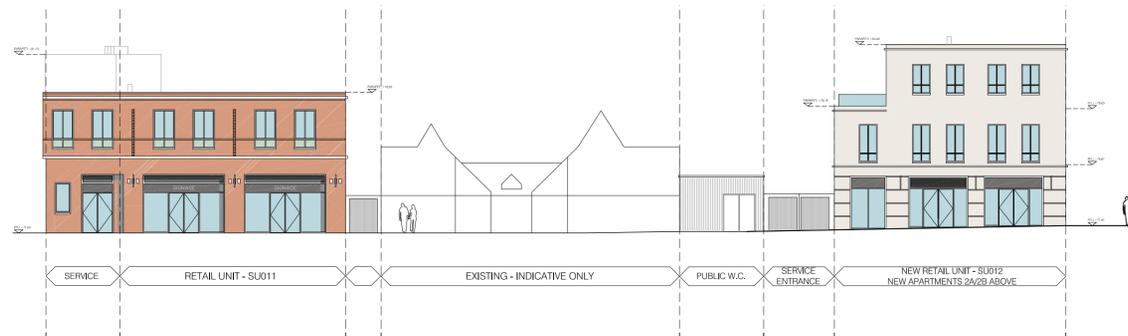
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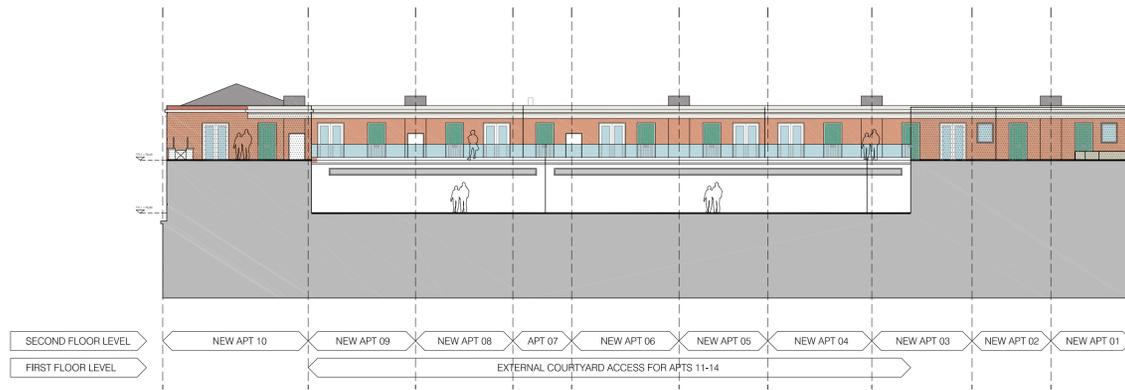
ELEVATION D-D\_NORTH PLAZA\_[SCALE 1:200 @ A1]



ELEVATION E-E\_EAST PLAZA\_[SCALE 1:200 @ A1]



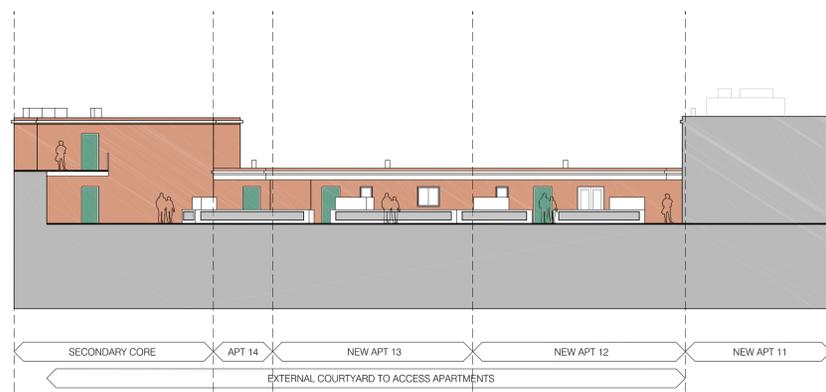
ELEVATION F-F\_SOUTH PLAZA\_[SCALE 1:200 @ A1]



PEACH STREET BUILDING INTERNAL COURTYARD\_[SCALE 1:200 @ A1]



ELEVATION G-G\_WEST PLAZA\_[SCALE 1:200 @ A1]

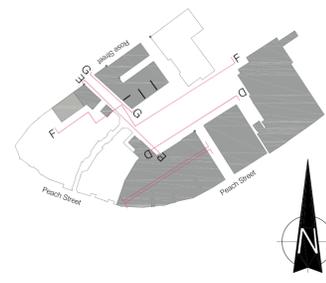


PEACH STREET BUILDING INTERNAL COURTYARD\_[SCALE 1:200 @ A1]

ELEVATION HATCH KEY

	_ RED BRICK		_ SIGNAGE / CLADDING
	_ RED BRICK SET BACK		_ ROOF TILE
	_ RENDER		_ GLAZING
	_ FROSTED GLAZING		

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A10	02.12.14	General Update	DRI	EP
A09	17.11.14	General Amendments - Draft	DRI	EP
A08	05.11.14	General Amendments - Draft	DRI	EP
A07	24.10.14	General Amendments - Draft	DRI	EP
A06	21.10.14	Amended East Plaza Elevation	DRI	EP
A05	15.10.14	General Update - Detail	DRI	EP
A04	17.09.14	General Update (inc new SU006)	DRI	EP
A03	04.09.14	New Scheme Update	DRI	EP
A02	10.09.13	General Update	DSh	EP
A01	08.08.13	Scheme Updates	TD	EP

A00 01.07.13 Drg no. BNY-PD-08-LL02 superseded to from BNY-PD-08-LL08 & LL09 RM EP

Rev	Date	Description	By	Chkd
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Design Architecture Masterplanning Interiors Graphics

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**Client**  
 WILSON BOWDEN DEVELOPMENTS LIMITED

---

**Project**  
 PEACH PLACE DEVELOPMENT WOKINGHAM

---

**Drawing Title**  
 PROPOSED ELEVATIONS

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<b>Drawn By</b> TD	<b>Checked By</b> EP
<b>Scale</b> 1:200 @ A1	<b>Date</b> AUGUST 2013

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<b>Project No.</b> 4141	<b>Drawing No.</b> BNY-PD-08-GE LL09 A10	<b>Revision</b>
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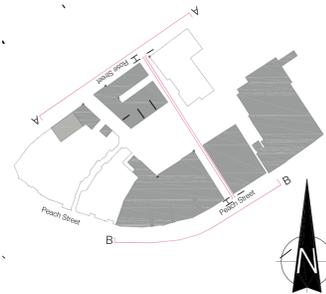
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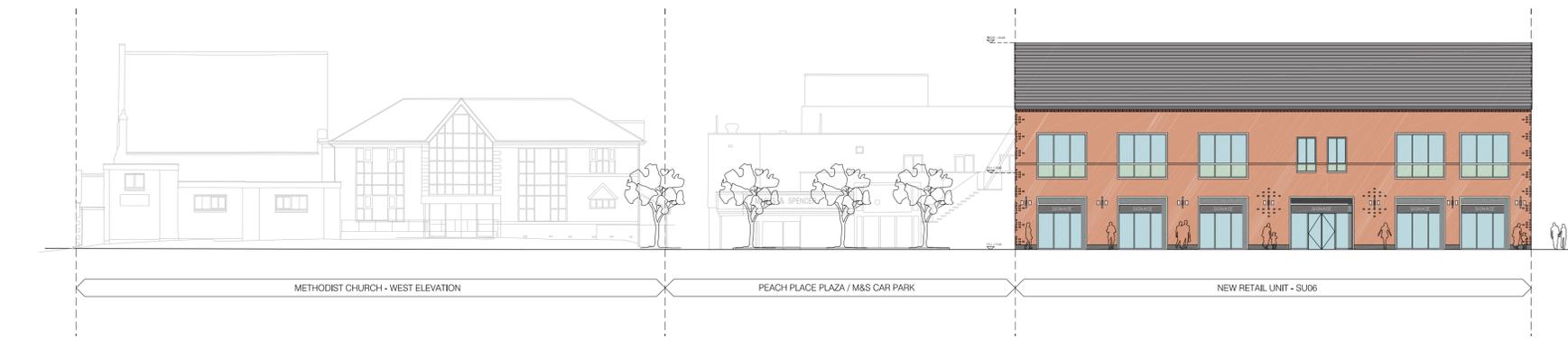
ELEVATION A-A, ROSE STREET [SCALE 1:200 @ A1]



ELEVATION B-B, PEACH STREET [SCALE 1:200 @ A1]



ELEVATION H-H, ARCADE [SCALE 1:200 @ A1]



ELEVATION I-I, ARCADE [SCALE 1:200 @ A1]

A09	02.12.14	General Update	DRI	EP
A08	17.11.14	General Update - Detail	DRI	EP
A07	11.11.14	General Update - Detail	DRI	EP
A06	05.11.14	General Update - Detail	DRI	EP
A05	24.10.14	General Update - Detail	DRI	EP
A04	15.10.14	General Update - Detail	DRI	EP
A03	04.09.14	New Scheme Update	DRI	EP
A02	10.09.13	General Update	DSh	EP
A01	08.08.13	Scheme Updates	TD	EP
A00	01.07.13	Drp no. BNY-PD-08-LL02 superseded to from BNY-PD-08-LL08 & LL09	RM	EP

Rev	Date	Description	By	Chkd
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**Benoy**  
 Design Architecture Masterplanning Interiors Graphics

Client  
**WILSON BOWDEN DEVELOPMENTS LIMITED**

Project  
**PEACH PLACE DEVELOPMENT WOKINGHAM**

Drawing Title  
**PROPOSED ELEVATIONS**

Drawn By  
 TD

Checked By  
 EP

Scale  
 1:200 @ A1

Date  
 AUGUST 2013

Project No.  
 4141

Drawing No.  
 BNY-PD-08-GE LL08 A09

Revision  
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Status  
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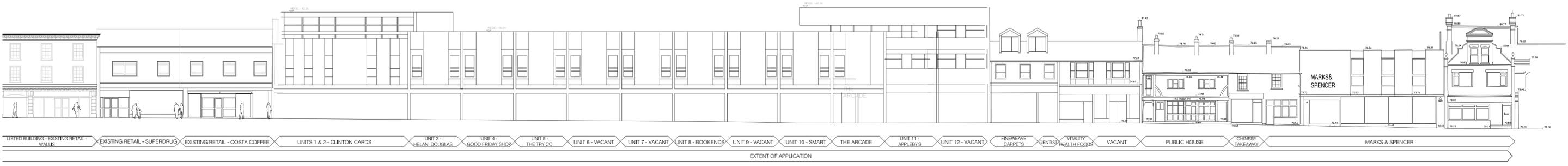
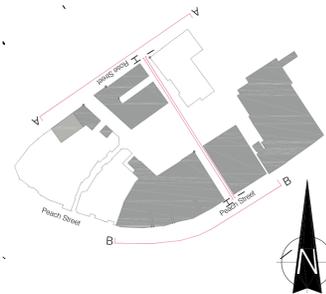
**ELEVATION HATCH KEY**

- \_ RED BRICK
- \_ RED BRICK SET BACK
- \_ RENDER
- \_ GLAZING
- \_ FROSTED GLAZING
- \_ SIGNAGE / CLADDING
- \_ ROOF TILE



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ELEVATION B-B\_Peach Street EXISTING\_ [SCALE 1:200 @ A1]



ELEVATION B-B\_Peach Street PROPOSED\_ [SCALE 1:200 @ A1]

--- ROOF LINE OF EXISTING PEACH STREET BUILDINGS

**ELEVATION HATCH KEY**

_ RED BRICK	_ SIGNAGE / CLADDING
_ RED BRICK SET BACK	_ ROOF TILE
_ RENDER	
_ GLAZING	
_ FROSTED GLAZING	

Rev	Date	Description	By	Chkd
A00	09.02.15	Initial Issue	DRI	EP

Benoy

Design Architecture Masterplanning Interiors Graphics

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**Client**  
WILSON BOWDEN DEVELOPMENTS LIMITED

---

**Project**  
PEACH PLACE DEVELOPMENT WOKINGHAM

---

**Drawing Title**  
PEACH STREET ELEVATION EXISTING/PROPOSED

---

<b>Drawn By</b> DRI	<b>Checked By</b> EP
<b>Scale</b> 1:200 @ A1	<b>Date</b> FEBRUARY 2015

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<b>Project No.</b> 4141	<b>Drawing No.</b> BNY-PD-08-GE LL11 A00	<b>Revision</b>
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3. All dimensions to be confirmed on site prior to construction.

4. All dimensions to be confirmed on site prior to construction.

5. All dimensions to be confirmed on site prior to construction.

6. All dimensions to be confirmed on site prior to construction.

7. All dimensions to be confirmed on site prior to construction.

8. All dimensions to be confirmed on site prior to construction.

9. All dimensions to be confirmed on site prior to construction.

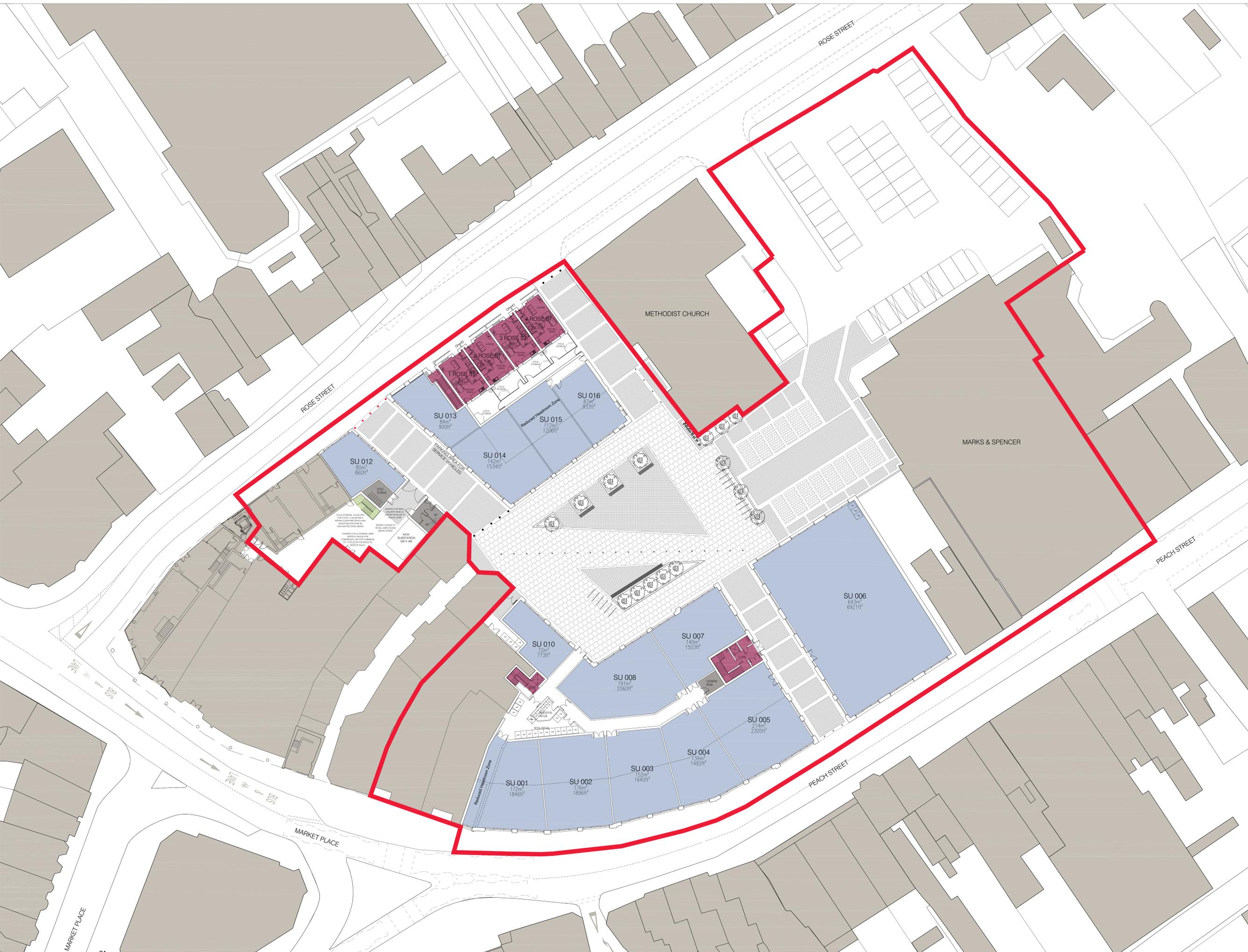
10. All dimensions to be confirmed on site prior to construction.



**MASTERPLAN KEY**

- APPLICATION BOUNDARY
- RETAIL UNIT
- RESIDENTIAL

**NOTE: SUB-DIVISION OF RETAIL UNITS IS FOR INDICATIVE PURPOSES ONLY**



A29	12.02.15	Cycle Storage Provision	DR	EP
A28	10.02.15	Cycle Storage Provision	DR	EP
A27	09.02.15	Note relating to sub-division of units	DR	EP
A26	08.02.15	Commercial Cycle Storage Layout	DR	EP
A25	06.01.15	MSS Boundary	DR	EP
A24	02.02.14	General Updates	DR	EP
A23	17.11.14	General Amendments - Draft	DR	EP
A22	05.11.14	General Amendments - Draft	DR	EP
A21	24.10.14	General Amendments - Draft	DR	EP
A20	23.10.14	General Amendments - Draft	DR	EP
A19	21.10.14	General Amendments - Draft	DR	EP
A18	15.10.14	General Amendments - Draft	EP	EP
A17	09.10.14	Amended Layout	EP	EP
A16	24.09.14	Amended Layout / Refuse Option - Draft	DR	EP
A15	23.09.14	Amended Unit Layout Option - Draft	DR	EP
A14	04.09.14	General Amendments - Draft	DR	EP
A13	14.08.14	General Amendments - Draft	DR	EP
A12	10.07.14	Amended Scheme - Draft	DR	EP
A11	12.11.13	Room names added to residential	EP	EP
A10	10.09.13	General Updates	DR	EP
A09	09.08.13	Scheme Updates	TD	EP
A08	01.02.13	Window added to Rose Street gable end	TH	EP
A07	24.01.13	Blue Badge parking spaces introduced	TH	EP
A06	18.01.13	MSS Car Park Layout	TH	EP
A05	14.01.13	General amendments	TH	EP
A04	22.11.12	Landscape & General Updates	TH	LC
A03	09.11.12	General Updates	TH	LC
A02	05.11.12	Existing Substation removed, 2' new added. Other general amendments	TH	LC
A01	31.10.12	SU04 Amended to sub boundary line	TH	LC



Design Architecture Masterplanning Interiors Graphics

Client

**WILSON BOWDEN DEVELOPMENTS LIMITED**

Project

**PEACH PLACE DEVELOPMENT WOKINGHAM**

Drawing Title

**PROPOSED GROUND FLOOR PLAN**

Drawn By: TD | Checked By: EP

Scale: 1:200 @ A0 | Date: AUGUST 2013

Project No: 4141 | Drawing No: BNY-PD-08-GP-0001 | Revision: A29

Status: **PLANNING**

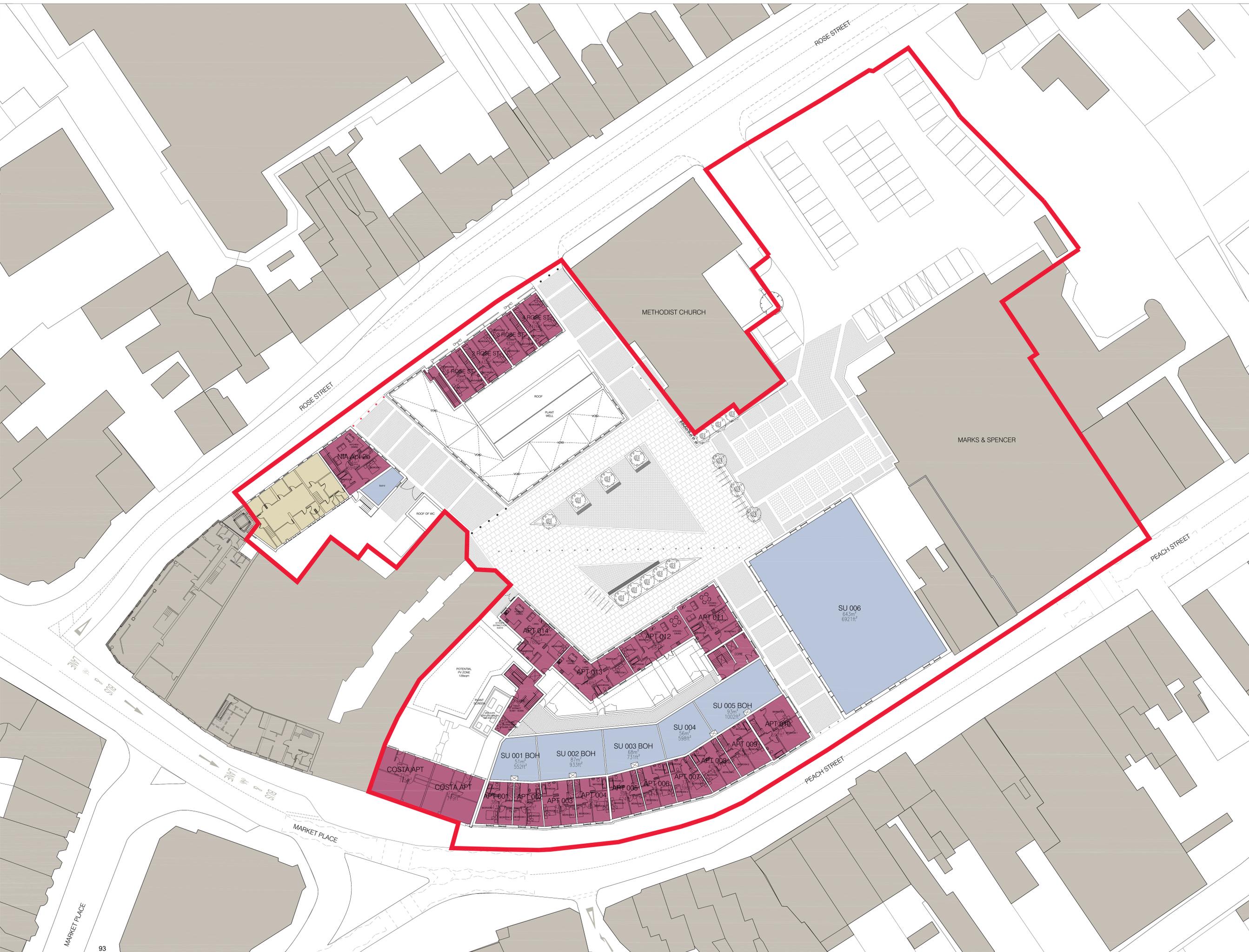


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- APPLICATION BOUNDARY
- STANDARD RETAIL UNIT
- RESIDENTIAL
- EXISTING RETAIL

NOTE: SUB-DIVISION OF RETAIL UNITS IS FOR INDICATIVE PURPOSES ONLY



Rev	Date	Description	By	CHKD
A21	12.02.15	PV Zone Amendment	DRI	EP
A20	09.12.15	Note relating to sub-division of units	DRI	EP
A19	02.12.15	General Update	DRI	EP
A17	05.11.14	General Amendments - Draft	DRI	EP
A16	24.10.14	General Amendments - Draft	DRI	EP
A15	15.10.14	General Amendments - Draft	DRI	EP
A14	29.09.14	Amended Apartment Layouts - Draft	DRI	EP
A13	04.09.14	General Amendments - Draft	DRI	EP
A12	14.08.14	General Amendments - Draft	DRI	EP
A11	15.07.14	Amended Scheme - Draft	DRI	EP

**Benoy**  
 Design Architecture Masterplanning Interiors Graphics

Client  
**WILSON BOWDEN DEVELOPMENTS LIMITED**

Project  
**PEACH PLACE DEVELOPMENT WORKINGHAM**

Drawing Title  
**PROPOSED FIRST FLOOR PLAN**

Drawn By: TD  
 Checked By: EP

Scale: 1:200 @ A0  
 Date: AUGUST 2013

Project No.: 4141  
 Drawing No.: BNY-PD-08-GP-1001  
 Revision: A21

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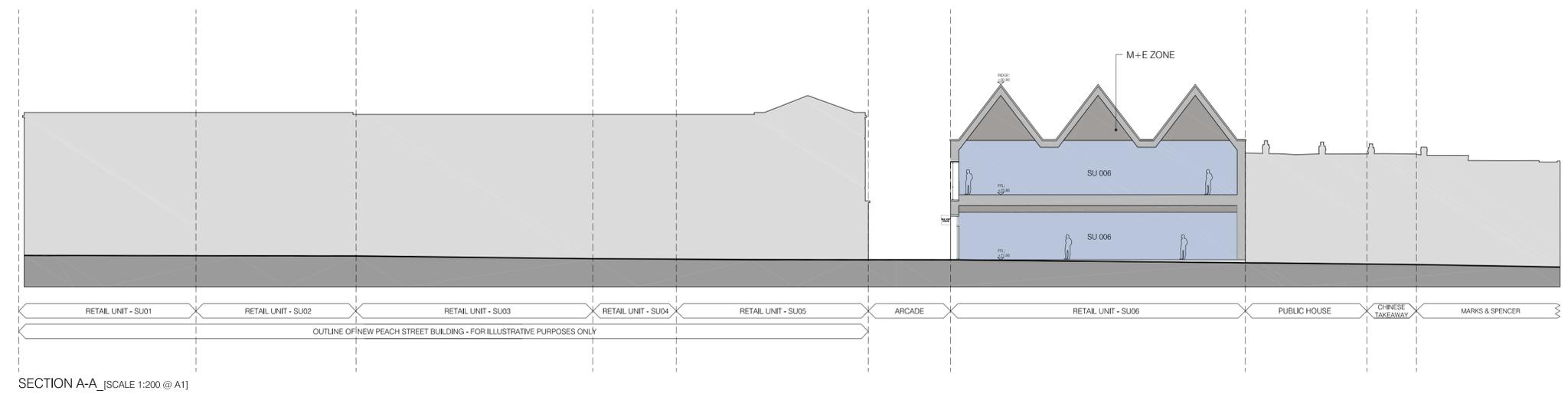
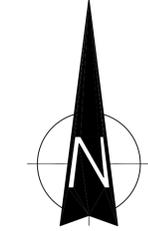
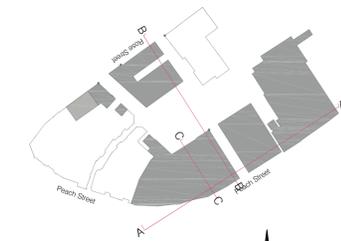
**Notes**

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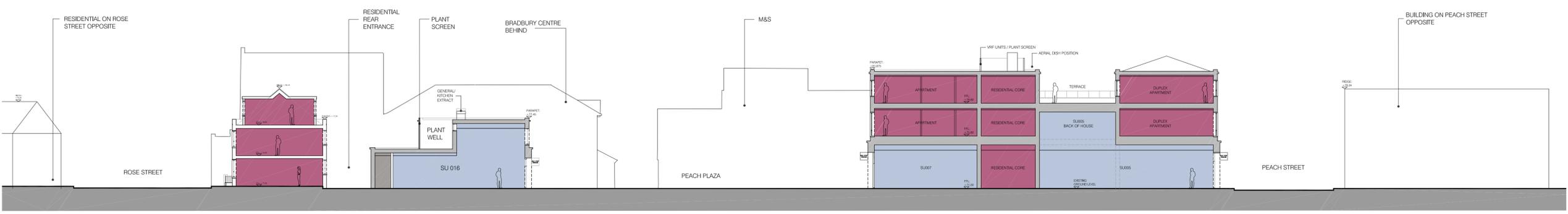
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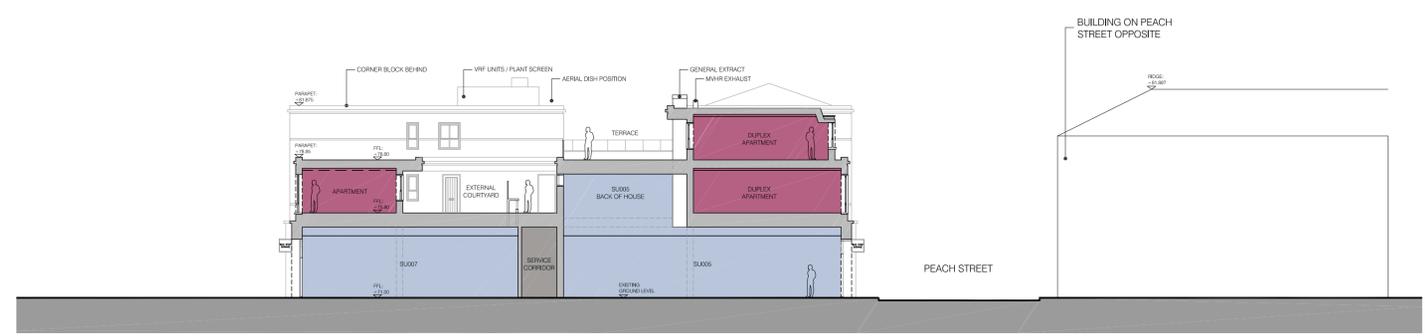
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SECTION A-A [SCALE 1:200 @ A1]



SECTION B-B [SCALE 1:200 @ A1]



SECTION C-C [SCALE 1:200 @ A1]

A10	28.11.14	General Update	DR	EP
A09	17.11.14	General Amendments/Updates	DR	EP
A08	05.11.14	General Amendments/Updates	DR	EP
A07	04.09.14	Updated Scheme	DR	EP
A06	14.07.14	Updated Scheme	DR	EP

Rev	Date	Description	By	Chk'd
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Client  
**WILSON BOWDEN DEVELOPMENTS LIMITED**

Project  
**PEACH PLACE DEVELOPMENT WOKINGHAM**

Drawing Title  
**PROPOSED SECTIONS**

Drawn By: TH  
Checked By: LC

Scale: 1:200 @ A1  
Date: AUGUST 2013

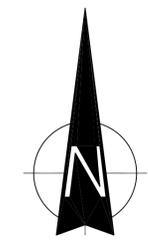
Project No.: 4141  
Drawing No.: BNY-PD-08-GS LL02 A10  
Revision:

Status  
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- MASTERPLAN KEY**
- \_ APPLICATION BOUNDARY
  - \_ RETAIL UNIT
  - \_ RESIDENTIAL
- SERVICING KEY**
- \_ PLAZA SERVICING
  - 1 \_ RETAIL UNITS
  - 2 \_ M&S SERVICING AS EXISTING
  - \_ BOLLARDS  
DROP 7AM - 10AM
  - (RESTRICTED ACCESS: 10AM - 11PM)
  - \_ BOLLARDS  
DROP 7AM - 10AM
  - ABSOLUTE EXCLUSION  
11PM - 7AM

A08	03.02.15	General Update	DRI	EP
A07	28.11.14	General Update	DRI	EP
A06	19.11.14	General Notes Added	DRI	EP
A05	17.11.14	New Scheme - Amended Servicing	DRI	EP
A04	11.09.13	General Update	DSH	EP
A03	08.08.13	Scheme Updates	TD	EP
A02	29.01.13	General amendments	TH	EP
A01	14.01.13	General amendments	TH	LC

Rev	Date	Description	By	Chkd

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**Client**  
 WILSON BOWDEN DEVELOPMENTS LIMITED

**Project**  
 PEACH PLACE DEVELOPMENT WOKINGHAM

**Drawing Title**  
 PEACH PLACE SERVICING

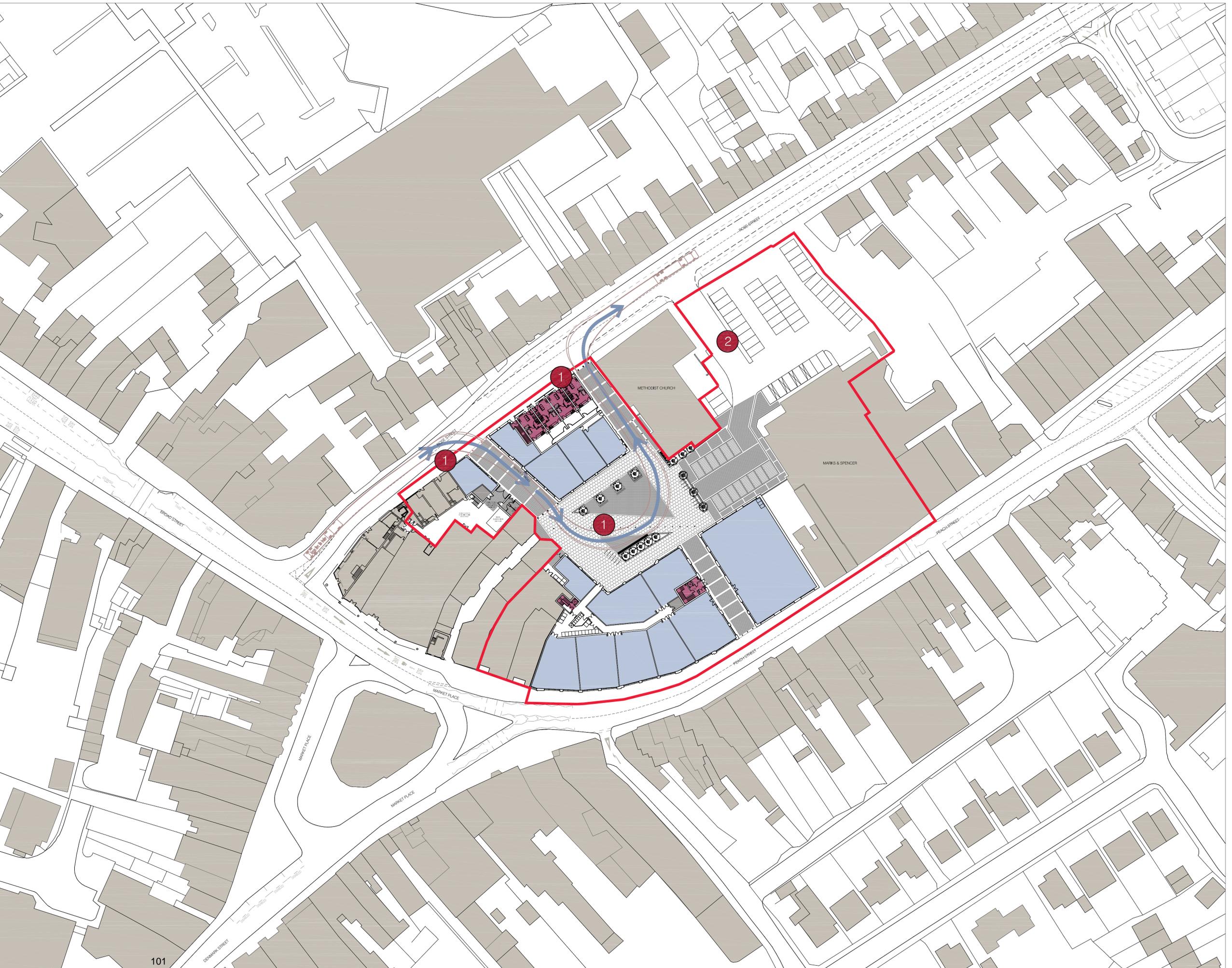
**Drawn By** TH      **Checked By** LC

**Scale** 1:500 @ A1      **Date** AUGUST 2013

**Project No.** 4141      **Drawing No.** BNY-PD-08-GPLL010A08      **Revision**

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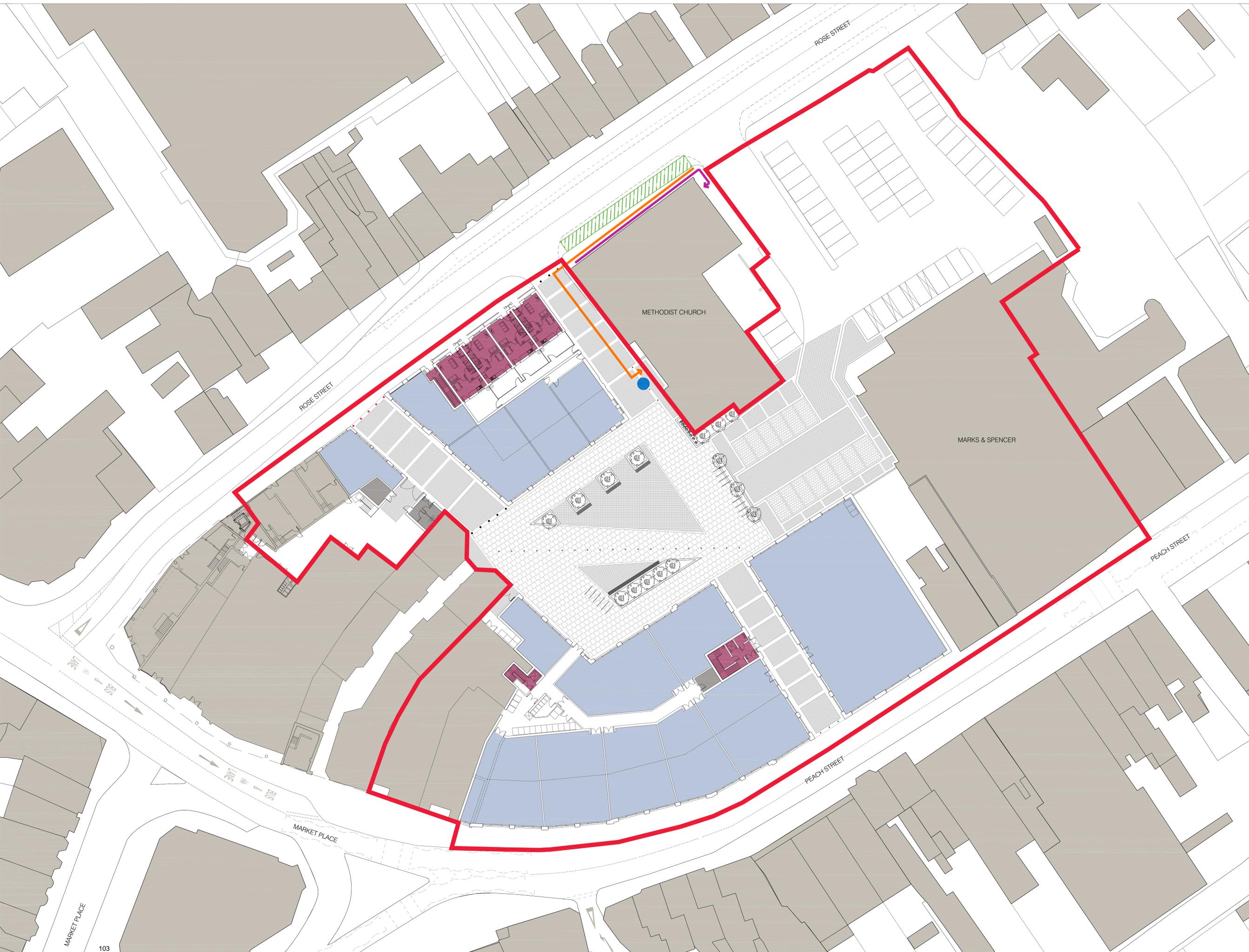




Notes  
 An application for a Disabled Person's Single Space (DPS) is required to be submitted with this plan.  
 The location of the proposed disabled parking bays is based on the current 1:200 scale plan and is subject to confirmation during the detailed design phase.  
 The proposed disabled parking bays are subject to the provisions of the Equality Act 2010. The proposed disabled parking bays are subject to the provisions of the Equality Act 2010. The proposed disabled parking bays are subject to the provisions of the Equality Act 2010.



- APPLICATION BOUNDARY
- RETAIL UNIT
- RESIDENTIAL
- AREA IDENTIFIED FOR PROVISION OF DISABLED PARKING BAYS
- BRADBURY CENTRE ENTRANCE  
GRADIENT = 1 IN 36
- FROM ROSE STREET TO THE ENTRANCE (LEVELS SUBJECT TO CONFIRMATION DURING DETAILED DESIGN BUT WOULD MEET MINIMUM REQUIREMENTS)
- DISTANCE TO BRADBURY CENTRE ENTRANCE = 51M
- DISTANCE TO METHODIST CHURCH ENTRANCE = 29M
- NOTE: DISTANCE TO ENTRANCES TAKEN FROM FURTHEST POINT OF THE PARKING BAYS



A00	05.02.15	Initial Issue	DR	EP
Rev	Date	Description	By	Check



Client  
**WILSON BOWDEN DEVELOPMENTS LIMITED**

Project  
**PEACH PLACE DEVELOPMENT WOKINGHAM**

Drawing Title  
**PROPOSED PROVISION OF DISABLED PARKING BAYS**

Drawn By: DRB  
 Checked By: EP

Scale: 1:200 @ A0  
 Date: FEBRUARY 2015

Project No.: 4141  
 Drawing No.: BNY-PD-08-GP-LL15  
 Revision: A00

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# WOKINGHAM TOWN COUNCIL

## Comments on Peach Place planning application F/2014/2637

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### 1. GENERAL

- 1.1 Generally, whilst we think that the proposal is an improvement on the application previously submitted, there are several areas where we have reservations. We have therefore concentrated on the areas where we believe that improvements could be made.

### 2. ARCHITECTURE

- 2.1 The triple gable façade of the proposed SU06 retail unit is too high, far too imposing and out of proportion to the rest of the redevelopment. As the roof space will not be used, we see no reason why the height cannot be reduced without compromising the design as a whole.
- 2.2 The attempt to break up the monolithic expanse of the gables by the apparently random scattering of brick projections is very poor.
- 2.3 The choice of a brick colour that veers more towards buff than red, when combined with a grey metal dormer cladding and roofing, creates a particularly drab appearance.
- 2.4 The bland monotony of the units in Peach Street could still be further improved, with more attention being paid to the depth of relief at window and door openings. The introduction of recessed and projecting band courses within the facades, together with more attention to contrasting corner detailing, would greatly improve the overall appearance and break up the distinctly “flat” feeling of these elevations.
- 2.5 We would prefer the flat roofs within Peach Place itself to be pitched, even if only slightly raised.
- 2.6 The slate roofs of the proposed town houses in Rose Street are completely out of keeping with the architectural style of the surrounding historic buildings. This feature, combined with the grey metal-clad dormer windows, produces a drab appearance more reminiscent of back-to-back workmen’s cottages in industrial areas of Lancashire. A simple glance across Rose Street shows predominantly red brick buildings with clay-tiled roofs and, where dormers are evident, the cheeks are clad in vertical clay tiling.
- 2.7 The monotonous uniformity of the roof appearance could be improved by introducing an extension of the party walls upwards into the roofline between units, with a brick parapet detail, accentuating the division between each property.
- 2.8 The dormer windows to the town houses in Rose Street appear to be completely out of proportion to the buildings.
- 2.9 In Peach Place in particular, but generally elsewhere, there appear to be ledges that will be very popular with pigeons. Either the ledges should be removed or anti-pigeon materials should be used from the outset.

### 3. PARKING

- 3.1 We remain surprised and frustrated at the complete absence of a comprehensive parking strategy for Wokingham Town to underpin this application. The Wokingham Town Car Parks Strategy, issued in September 2013, was rapidly withdrawn after public consultation and nothing more has been heard since then.

- 3.2 This frustration is compounded by our understanding that Wokingham Borough Council apparently intends that off-street parking and on-street parking are to be dealt with as separate matters, when it is clear to most people that the two are intrinsically linked. We question how a holistic parking strategy for the Town can fail to recognise this and we cannot understand how this scheme has been developed to the degree that it has without such a strategy being in place.
- 3.3 The Transport Assessment states (paragraph 6.2.15) that *“The car park occupancy data utilised for this application has been collected by Wokingham Borough Council Parking team to inform the Council’s Wokingham Off-Street Car Park Capacity Statement. This statement will be published early in 2015 and the findings of this assessment have been informed by the draft statement and its conclusions”*. However, as the Wokingham Off-Street Car Park Capacity Statement hasn’t yet been published, we are therefore unable to test the validity of the assumptions made about parking, on which the Transport Assessment relies.
- 3.4 We take issue with the *“Parking provision required to support the proposed commercial floorspace”* contained within the Transport Assessment (TA), in particular paragraphs 6.2.3 - 6.2.29, for the following reasons.
- 3.4.1 Paragraph 6.2.4 of the TA states that *“Parking requirements for new schemes are identified using the parking standards contained with the Council’s Managing Development Delivery Plan (adopted 2014)”*.
- 3.4.2 The parking standards contained with the Council’s MDD Plan are *“based on the evidence and research presented in the Parking Standards Study Report 2011”*. (MDD: Appendix 2: Car Parking Standards: paragraph 1.0.2).
- 3.4.3 *“Section 5 - Recommended Parking Standards”* of the Parking Standards Study Report 2011 sets out Use Classes and states that *“the standards are based on the evidence and research presented in previous chapters within this report”*.
- 3.4.4 However, the only apparent *“evidence and research”* in respect of Retail Parking presented in previous chapters of WSP’s Parking Standards Study Report 2011 comprises the following three paragraphs, quoted verbatim:
- “3.6 RETAIL PARKING*
- 3.6.1 There is a wide diversity of parking that can take place for retail uses. Food and non-food uses can generate different levels of demand and can therefore be treated separately. Retail bulk goods such as supplied at garden centres or DIY stores also attract more parking due to heavier take away goods, and changes in facilities, discussed below, can lead to longer durations of stay (affecting parking accumulations).*
- 3.6.2 The increase in provisions of cafes and restaurants in DIY and garden centres also has the effect of increasing dwell times and affecting parking accumulations. It is suggested that careful consideration is given to planning applications which seek to make internal changes to these types of retail use and the implications for parking, especially at busy periods such as bank holidays. In these instances, careful management of the site will be required and it is suggested that conditions are imposed that require management plans for different scenarios to be provided at the planning stage.*

3.6.3 *In retail centres such as those in Wokingham and Woodley, parking is provided centrally in public car parks. It is assumed that any new non-food retail development located within 250 metres of a public car park in Wokingham town centre will have its customer car parking provided centrally in this way. The proposed parking zone is illustrated in Figure 3.1 below. A similar proposed zone is illustrated in Woodley.”*

- 3.4.5 These paragraphs hardly appear to define a conclusive, evidence-based, foundation on which to found a retail parking strategy for a major town centre development.
- 3.4.6 Paragraph 6.2.7 of the TA then assumes the most advantageous use class of A1 - Non-Food Retail, a decision taken “In agreement with the Local Planning Authority”. Is this a pre-determined decision and, if so, on what evidence has it been made?
- 3.4.7 Paragraph 6.2.8 of the TA states that “As the exact location of A3-A5 uses within the scheme is not yet known, it would be impossible to assume a realistic floorspace calculation to accurately apply the standard at this stage”. If so, what validity can be put on the assumptions made?
- 3.4.8 All of the preceding argument is then justified in paragraph 6.2.9, as follows: “This flexibility in applying the standards is in line with paragraph 2.51 of the MDD set out above, which acknowledges that the standards allow for flexibility in their application to accommodate the specific circumstances of development proposals.” In other words, as it’s impossible to accurately apply the MDD parking standards, the MDD allows sufficient flexibility to make a series of highly generalised assumptions, without apparent foundation or evidence to support them.
- 3.4.9 Paragraph 6.2.10 gives a figure of 3,674m<sup>2</sup> (GFA) of new commercial A1-A5 floorspace at Peach Place. Paragraph 6.2.11 then deducts the existing commercial floorspace from the new, to identify the parking requirement generated by the net increase in retail floorspace. However, if the existing commercial floorspace already is served by the existing car parking on site, the required parking has to serve the new floorspace of 3,674m<sup>2</sup>, not the net increase.
- 3.4.10 It might perhaps have escaped the Consultants’ attention that the 50% of the Easthampstead Road Car Park mostly unoccupied is that part located over 250m from the centre. This is the reason that it’s not used and will continue not to be used. As 250m is the criterion assumed by WSP in their 2011 Parking Standards Study for the supply of parking to support Retail Development (see paragraph 3.4.4, item 3 above), the assumption that the spare capacity of the Easthampstead Road car park would absorb 83 of the 145 replacement spaces needed is therefore a highly optimistic assumption which actual current experience contradicts.
- 3.4.11 The assumption in paragraph 6.2.22 that the Waitrose car park can absorb the displacement of 26 cars from Peach Place is flawed; this is restricted to Waitrose customers only and has a maximum two hour restriction. Has anybody actually asked Waitrose whether they would agree to this?
- 3.4.12 The conclusion in paragraph 6.2.24 that the remaining 36 required spaces (25% of the total of 145) can simply be “*accommodated within existing town centre car parks*” is completely unsubstantiated conjecture.

3.4.13 The conclusion in paragraph 6.2.27 that “This section demonstrates that the parking requirement generated by the scheme proposals and the need to accommodate loss of parking in Rose Street car park can be accommodated within existing car parks in the town centre” has patently not been demonstrated.

3.5 Our observations are that:

3.5.1 Short term parking should be on-street, whilst off-street should be reserved for medium/longer term. There is also a demonstrable need for some free parking (e.g. the first hour free) to allow residents to “pop-in” for tasks such as visiting the bank, etc.

3.5.2 There is a pressing need to improve the multi-storey car park above Argos, which is under-used because it’s in such a dreadful state.

3.5.3 We are concerned that the new residential provision, which has no dedicated parking, will increase the pressure in the town generally.

3.5.4 We are concerned about the difficulty of residents on the north side of the town accessing parking which is predominantly in the south and therefore requires crossing the town, thereby increasing congestion.

3.5.5 We are very concerned that there will be no provision for residents' parking in Rose Street and the option offered that they can pay to park in a WBC car park. Why is it assumed that the new residents will not be car drivers?

3.5.6 The fact that Wokingham has several major housing developments in the SDLs, which we hope will bring many additional shoppers into the town centre, has largely been ignored. Whilst it may be the case that the existing car parks have spare capacity and can cope with the displaced 99 spaces (although this hasn't yet been demonstrated by WSP and we would not include Waitrose in this calculation), the capacity of the car parks, ease of access to residents in the south of Wokingham, differentiation between short stay, medium stay, and long stay, along with the charging policy, have yet to be agreed. This means looking at the Virgin Active car park as well as the Easthampstead Road and rear of Denmark Street facilities. The car parking strategy must be an integral part of the overall development.

#### **4. ACCESS**

4.1 Traffic flow would be improved if Wilshire and Rectory Roads could become two-way, even more positively if an area could be obtained from All Saints Church to widen the junction by The Ship.

4.2 The Transport Statement anticipates an increase in cyclists but does not provide many cycle racks other than for Peach Place residents.

4.3 The bus services are emphasised but the plans envisage that buses coming along Broad Street will still be able to go round the Market Place and then down Denmark Street. We understood that, with the redevelopment of the Market Place, this option was to be removed.

- 4.4 It concerns us that pedestrians will still have narrow pavements, which barely allow for a couple to walk side by side and, when they do, no-one can pass them. This is not conducive to family shopping and browsing. The solution of making Peach Street one lane and thus allowing the pavements to be widened has not been discussed. There is an incorrect statement that double yellow lines stop any parking. Lorries regularly park and unload in Peach Street, causing major traffic flow disruption. This problem must be eliminated.

## **5. SERVICING**

- 5.1 We are concerned that there will only be control over those outlets leased from WBC. It will therefore still remain possible for other outlets to continue to be serviced from the front, along Peach Street, which will mean that traffic flow will continue to be adversely affected. Enforcement is needed in relation to deliveries at the front and careful consideration also needs to be given to the timing of rear access deliveries. We wish it to be clear that Peach Street itself should be considered as a fully integrated part of the overall scheme, not just peripheral to the development.
- 5.2 We are concerned that the size of the service area for both retail and domestic uses appears to be insufficient.
- 5.3 Mention is made of the controls that will exist for delivery vehicle access to Peach Place, but there is no apparent effort to improve delivery access to the Peach Street shops. There is a service road (Goatley Way) but larger lorries coming along Peach Street are unable to turn into the entrance of the road. One option is that access should be from Sturges Road and the no entry sign into Goatley Way be removed.
- 5.4 We question whether traders' access to the rear of the shops is sufficient?

## **6. EVENTS**

- 6.1 Peach Place would clearly be a highly suitable area for events, markets, music, etc. It's therefore vital that the design of the central area be made as flexible as possible to accommodate such functions.
- 6.2 With this in mind, it's essential that the design incorporates the following features:
- 6.2.1 A suitable network of power, lighting, and water distribution points, together with appropriate drainage.
- 6.2.2 Adequate facilities and space layout to erect staging for events.
- 6.2.3 The incorporation of bolts and eyes into the fascias of the buildings, so that canopies, banners, wires for lights, Christmas lights, netting, etc. may be strung above the area.

## **7. RETAIL MIX**

- 7.1 Could consideration be given to encouraging Boots to take a second floor and become a bigger (almost 'anchor store') which would potentially attract other retailers?
- 7.2 Has sufficient thought been given to the changing pattern of shopping, given the growth of the internet? Will the town be overprovided with retail units? These should be planned with the greatest flexibility so that future conversion is possible. The shopping mix needs to be given very careful thought.

<b>Development Management Ref No</b>	<b>No weeks on day of committee</b>	<b>Parish</b>	<b>Ward</b>	<b>Listed by:</b>
F/2014/2105	21/21	Woodley	Bulmershe & Whitegates	Scheme of Delegation

**Applicant Location**      Bloor Homes Ltd & ABF Grain Products Ltd  
 Former Allied Bakeries site,      **Postcode** RG5 4BJ  
 Viscount Way,  
 Woodley,  
 Wokingham

**Proposal**      Proposed erection of 68 dwellings with associated roads, parking, amenity space, landscaping and creation of new access onto Loddon Bridge Road.

**Type**      Full  
**PS Category**      7  
**Officer**      David Wetherill

**FOR CONSIDERATION BY**      Planning Committee on 04/03/15  
**REPORT PREPARED BY**      Head of Development Management and Regulatory Services

**SUMMARY**

The application is a full application and proposes 68 dwellings with associated roads, parking, amenity space, landscaping. The site was included in policy SAL02 of the Managing Development Delivery Local Plan (MDD) as an allocated housing site for the development of around 57 dwellings.

The site is within walking distance of Woodley Town Centre, and the development would replace an industrial scale bakery which closed in 2006. The proposed number of dwellings represents an appropriate density of development (32.4 dph) providing space for adequately sized gardens and adequate landscaping within the site.

The application is a resubmission of withdrawn F/2014/1107, which proposed 73 dwellings. This previous scheme was considered to represent overdevelopment of the site, evident in factors such as the substandard gardens proposed. In this current scheme, all plots meet meet the minimum recommended 11m garden depth except three corner plots. However these three corner plots are wider than average and are therefore considered acceptable.

Access to the site would be through a newly formed access from Loddon Bridge Road. The application has attracted a number of local objections, principally in respect of this proposed new access. However the Highway Authority considers the safety of the access and traffic implications to be acceptable.

The site adjoins commercial uses to the north of the site, and the proposed mitigation measures ensure an acceptable noise environment for the proposed residential development, whilst also building in safeguards for the future should activity change in nature or increase in activity.

It is considered that the scheme is compliant with the development plan and as such is

recommended for conditional approval.

This application is before the Planning Committee as it is major development. The proposals are considered acceptable and therefore approval is recommended subject to conditions and the completion of a satisfactory Section 106 agreement.

### PLANNING STATUS

- Allocated Housing Site
- Major Development Location
- Contaminated Land Consultation Zone
- Bridleway (adjacent to site)
- Tree Preservation Orders
- Flood Zone 1

### RECOMMENDATION

**That the committee authorise the GRANT OF PLANNING PERMISSION by the Head of Development, subject to the prior completion of a satisfactory s.106 agreement, and conditions as follows:**

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

*Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).*

2. Before the development hereby permitted is commenced, samples and details of the materials to be used in the construction of the external surfaces of the buildings shall have first been submitted to and approved in writing by the Local Planning Authority. Development shall be carried out in accordance with the approved details.

*Reason: To ensure that the external appearance of the development is satisfactory.  
Relevant policies: Core Strategy policies CP1 and CP3*

3. This permission is in respect of the following drawings:

Drawing Title	Drawing Number		Revision
Site Layout	SO-003	SL-100	E
Site Layout-col	SO-003	SL-100	E
Parking Strategy- col	SO-003	SL-101	C
Storey Height Layout- col	SO-003	SL-102	C
A.H Tenure Layout	SO-003	AH-100	B
Apart Elevation & GF Plan Plots 14-22	SO-003	PD-103	
Apart Elevation & FF Plan Plots 14-22	SO-003	PD-104	
Apart Elevation & SF Plan Plots 14-22	SO-003	PD-105	
2B4P Elevations Plots 8,9,10,11	SO-003	PD-106	
2B4P Floor Plans Plots 8, 9, 10, 11	SO-003	PD-107	
3B5P Elevations & Floor Plans Plots 12,	SO-003	PD-108	

13			
303sp Elevations Plots 6, 7, 26, 27, 44, 45, 51, 52	SO-003	PD-109	
303sp Floor Plans Plots 6, 7, 26, 27, 44, 45, 51, 52	SO-003	PD-110	
303sp Elevations Plots 28,29, 47, 48, 49, 50	SO-003	PD-111	
303sp Floor Plans Plots 28,29, 47, 48, 49, 50	SO-003	PD-112	
2B4P & 4B6P Elevation Plots 36, 37, 38,	SO-003	PD-113	A
2B4P & 4B6P Floor Plans Plots 36, 37, 38	SO-003	PD-114	
2B4P & 4B6P Elevation Plots 39, 40	SO-003	PD-115	A
2B4P & 4B6P Floor Plans Plots 39, 40	SO-003	PD-116	
419 Elevations & Floor Plans Plot 65	SO-003	PD-117	
419 Elevations & Floor Plans Plot 67	SO-003	PD-118	
419 Elevations & Floor Plans Plots 59, 64	SO-003	PD-119	
412 Elevations & Floor Plans Plot 68	SO-003	PD-120	
313 Elevations & Floor Plans Plots 3, 4	SO-003	PD-121	
427 Elevations Plot 31	SO-003	PD-122	
427 Floor Plans Plot 31	SO-003	PD-123	
427 Elevations & Floor Plans Plots 61, 62	SO-003	PD-124	
350 & 351 Elevation & GF Plan Plots 53-55	SO-003	PD-125	
350 & 351 Elevation & FF Plan Plots 53-55	SO-003	PD-126	
350 & 351 Elevation & SF Plan Plots 53-55	SO-003	PD-127	
303 Elevations Plots 57, 58	SO-003	PD-128	
303 Floor Plans Plots 57, 58	SO-003	PD-129	
406 Elevations & Floor Plans Plots 60, 63	SO-003	PD-130	
406 Elevations & Floor Plans Plot 32	SO-003	PD-131	
406 Elevations & Floor Plans Plot 1	SO-003	PD-132	
411 Elevations & Floor Plans Plot 66	SO-003	PD-133	
400sp Elevation & Floor Plan Plots 23, 24	SO-003	PD-134	
410 Elevation & Floor Plans Plots 25	SO-003	PD-135	
350 Elevations Plots 33, 34, 35	SO-003	PD-136	
350 Floor Plans Plots 33, 34, 35	SO-003	PD-137	
313 Elevations & Plans Plot 5	SO-003	PD-138	
309 Elevations & Floor Plans Plot 2	SO-003	PD-139	
309 Elevations & Floor Plans Plot 46	SO-003	PD-140	
310 Elevations & Floor Plans Plot 43	SO-003	PD-141	
Single & Double Garage	SO-003	PD-142	
Single & Double Garage (XL)	SO-003	PD-143	
Elevation & Floor Plan- Cycle Store	SO-003	PD-144	
313 Elevations & Floor Plans Plot 30	SO-003	PD-145	
501 Elevations Plot 56	SO-003	PD-146	
501 Floor Plans Plot 56	SO-003	PD-147	
303sp Elevations Plots 41, 42	SO-003	PD-148	
303sp Floor Plans Plots 41, 42	SO-003	PD-149	
Street Scene 1	SO-003	SC-150	
Street Scene 2	SO-003	SC-151	

The development hereby permitted shall be carried out in accordance with the approved

details unless otherwise agreed in writing by the Local Planning Authority.

*Reason: To ensure that the development is carried out in accordance with the application form and associated details hereby approved.*

4. Prior to the commencement of the development a landscape management plan, including long term design objectives, management responsibilities, timescales and maintenance schedules for all landscape areas, other than privately owned, domestic gardens, shall be submitted to and approved in writing by the local planning authority. The landscape management plan shall be carried out as approved.

*Reason: In order to ensure that provision is made to allow satisfactory maintenance of the landscaping hereby approved.*

*Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21*

5. a) The tree protection measures shall be implemented in complete accordance with the Approved Scheme (Prepared by ACD Arboriculture received by the LPA on 24/09/14) for the duration of the development (including, unless otherwise provided by the Approved Scheme) demolition, all site preparation work, tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery.

b) No development (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) shall commence until the local planning authority has been provided (by way of a written notice) with a period of no less than 7 working days to inspect the implementation of the measures identified in the Approved Scheme on-site.

c) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.

d) The fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval of the local planning authority has first been sought and obtained.

*Reason: To secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area.*

*Reason: In the interests of visual amenity.*

*Relevant policies: NPPF, Core Strategy policy CP3 and MDD Policy TB21.*

6. Prior to the commencement of the development, full details of both hard and soft landscape proposals shall be submitted to and approved in writing by the local planning authority. These details shall include, as appropriate, proposed finished floor levels or

contours, means of enclosure, car parking layouts, other vehicle and pedestrian access and circulation areas, hard surfacing materials and minor artefacts and structure (e.g. furniture, play equipment, refuse or other storage units, signs, lighting, external services, etc). Soft landscaping details shall include planting plan, specification (including cultivation and other operations associated with plant and grass establishment), schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate, and implementation timetable.

All hard and soft landscape works shall be carried out in accordance with the approved details prior to the occupation of any part of the development or in accordance with a timetable approved in writing by the local planning authority. Any trees or plants which, within a period of five years after planting, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.

*Reason: In the interests of visual amenity.*

*Relevant policies: NPPF, Core Strategy policy CP3 and MDD Policy TB21.*

7. Before the development hereby permitted is commenced details of all boundary treatments shall first be submitted to and approved in writing by the Local Planning Authority. The scheme of boundary treatments shall include details of a semi-permeable boundary to abut the bridleway. The approved scheme shall be implemented prior to the first occupation of the development or phased as agreed in writing by the Local Planning Authority. The scheme shall be maintained in the approved form for so long as the development remains on the site.

*Reason: To safeguard amenity and highway safety.*

*Relevant policies: NPPF, Core Strategy policies CP1, CP3 and CP6*

8. A 3m high acoustic fence to the northern boundary of the development site shall be installed between points A and B as shown on plan no SL-100E, before any of the new dwellings on the site are occupied. The applicant shall ensure that the acoustic fence is retained and maintained for the duration of the proposed development.

*Reason: To protect the amenity of future residents of the development.*

*Relevant policies: NPPF, Core Strategy policies CP1 and CP3*

9. Before the development hereby permitted commences the applicant shall submit to the local planning authority for written approval details of the specification of windows to be installed at various location across the site to ensure that the internal noise level in habitable rooms does not exceed 35dB<sub>L<sub>Aeq</sub> 16hr</sub> during the day and 30dB<sub>L<sub>Aeq</sub> 8hr</sub> at night. Details of alternative ventilation to be installed shall also be provided. The agreed glazing and ventilation details shall be installed and maintained on site at all times.

*Reason: To protect future residents from noise arising from neighbouring existing commercial and industrial uses*

*Relevant policies: NPPF, Core Strategy policies CP1 and CP3*

10. Before the development hereby permitted commences the applicant or their agents shall submit to the Local Planning Authority for written approval a Construction Environmental Management Plan (CEMP)

*Reason: To protect the amenity of local residents (and future residents of the site if*

*there is a phased release), from various environmental impacts arising during construction such as noise, vibration, odour, dust, air quality etc.*

*Relevant policies: NPPF, Core Strategy policies CP1 and CP3*

11. No work relating to the development hereby approved, including works of demolition or preparation prior to building operations, shall take place other than between the hours of 7.30 am and 18.00 pm Monday to Friday and 8.00 am to 13.00 pm Saturdays and at no time on Sundays or Bank or National Holidays unless otherwise agreed in writing with the Local Planning Authority.

*Reason: To safeguard amenity of neighbouring properties.*

*Relevant policies : NPPF, Core Strategy policies CP1 and CP3*

12. A) Prior to the commencement of development, a detailed remediation scheme shall be submitted to and approved in writing by the Local Authority.

B) The approved remediation scheme must be carried out in accordance with its terms prior to the commencement of development (other than that required to carry out remediation) unless otherwise agreed by the Local Planning Authority. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works.

C) Prior to occupation of the development, a validation report that demonstrates the effectiveness of the remediation carried out, shall be submitted to and approved in writing by the Local Authority.

D) In the event that contamination is found at any time when carrying out the approved development that was not previously identified, the Local Authority should be notified in writing within 3 working days. An investigation and risk assessment must be undertaken and where remediation is necessary a remediation scheme must be prepared which is subject to the approval in writing of the Local Planning Authority. Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority.

*Reason: To ensure that any contamination on the site is remediated to protect the existing/proposed occupants of the application site and adjacent land*

13. Before any development commences a further assessment of the groundwater contamination identified in Hydrock's report "Desk Study and Ground Investigation, Former Allied Bakeries, Reading dated April 2013 (Ref: R/13089/001 shall be undertaken, submitted to and approved in writing by the local planning authority. The assessment shall include the use of Detailed Quantitative Risk Assessment (DQRA) techniques to assess the risks posed by individual contaminants to nearby controlled water resources and outline any remediation measures to be taken to avoid risk when the site is developed. No building shall be occupied until the measures have been carried out and a validation report has been submitted to and approved in writing by the local planning authority

*Reason: To ensure that any contamination on the site is remediated to protect controlled water resources.*

14. Notwithstanding the provisions of the Town and Country Planning, (General Permitted Development) Order 1995 (as amended) (or any Order revoking and re-enacting that Order with or without modification), no additional windows or similar openings shall be constructed in the flank elevations in the first floor levels or above of

the buildings hereby permitted except for any which may be shown on the approved drawings.

*Reasons: To safeguard the residential amenities of neighbouring properties.*

*Relevant policies: Core Strategy CP3*

15. Those windows shown on the approved site layout drawing as having obscured glazing (all bathrooms & toilet windows, plus on side windows where shown) shall be permanently so-retained. The windows shall be non-opening unless the parts of the window which can be opened are more than 1.7 metres above the finished floor level of the room in which the window is installed and shall be permanently so-retained.

*Reason: To safeguard the residential amenities of neighbouring properties.*

*Relevant policy: Core Strategy policy CP3*

16. A) The dwellings shall seek to achieve Code Level 4 in accordance with the requirements of the Code for Sustainable Homes: Technical Guide [or such national measure of sustainability for house design that replaces that scheme]. No dwelling shall be occupied until a Final Code Certificate has been issued for it by an accredited assessor certifying that at Code Level 4 has been achieved.

B) If it is intended that the houses be built to less than Code level 4, full details of why Code Level 4 is not achievable on site must be provided to the Local Authority and approved in writing. Should following receipt of information the LPA agree to a lower code level, then no dwelling shall be occupied until a Final Code Certificate has been issued for it by an accredited assessor certifying that the agreed level has been achieved.

*Reason: To ensure developments contribute to sustainable development.*

*Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policy CC04*

17. The measures for generating 10% of the predicted energy requirement of the development from decentralised renewable and/or low carbon sources outlined in the submitted Energy Statement shall be implemented before the development is first occupied and shall remain operational for the lifetime of the development.

*Reason: To ensure developments contribute to sustainable development.*

*Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policy CC05*

18. No development shall take place until a measured survey of the site and a plan prepared to scale of not less than 1:500 showing details of existing and proposed finished ground levels (in relation to a fixed datum point) and finished floor levels shall be submitted to and approved in writing by the Local Planning Authority, and the approved scheme shall be fully implemented prior to the occupation of the building(s).

*Reason: In order to ensure a satisfactory form of development relative to surrounding buildings and landscape.*

*Relevant policies: NPPF, Core Strategy policies CP1 and CP3*

19. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 1995 (or any order revoking and re-enacting that order with or without modification) no buildings, extensions, or alterations permitted by Classes (A, B, C, D and E) of Part 1 of the Second Schedule of the 1995 Order shall be carried out.

*Reason: To safeguard the amenities of the occupiers of neighbouring properties, and preserve the character of the development. Plots are generally at or around minimum separation distances and amenity space sizes and so the Council wishes to assess the potential impact of such development.*

*Relevant policies: NPPF, Wokingham Borough Core Strategy Policy CP1, CP3 and the Wokingham Borough Council Borough Design Guide.*

20. No development shall take place until the applicant or their agents or successors in title have secured the implementation of a programme of archaeological work (which may comprise more than one phase of work) in accordance with a written scheme of investigation, which has been submitted to and approved in writing by the local planning authority.

*Reason: The site lies in an area of archaeological potential, particularly in relation to prehistoric remains.*

*Relevant Policy: NPPF, MDD TB25*

21. Before development commences, full details of the construction of roads and footways, including levels, widths, construction materials, depths of construction, surface water drainage and lighting shall be submitted to and approved in writing by the local planning authority. The roads and footways shall be constructed in accordance with the approved details to binder course level before the buildings are commenced and the final surface course shall be provided within 3 months of occupation, unless otherwise agreed in writing by the local planning authority.

*Reason: To ensure that roads and footpaths are constructed to a standard that would be suitable for adoption as publicly maintainable highway, in the interests of providing a functional, accessible and safe development. Relevant policy: Core Strategy policies CP3 & CP6.*

22. No development shall take place, including any works of demolition, until a Construction Method Statement has been submitted to, and approved in writing by, the local planning authority. The approved Statement shall be adhered to throughout the construction period. The Statement shall provide for:

- i) the parking of vehicles of site operatives and visitors,
- ii) loading and unloading of plant and materials,
- iii) storage of plant and materials used in constructing the development,
- iv) measures to prevent the deposit of materials on the highway and details of wheel washing facilities
- v) measures to inform local residents of the commencement of development by letter and provide appropriate contact details for residents to contact the developer if they have concerns or issues.

*Reason: In the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 & CP6.*

23. No part of any buildings hereby permitted shall be occupied or used until the vehicle parking space has been provided in accordance with the approved plans. The vehicle parking space shall be permanently maintained and remain available for the parking of vehicles at all times.

*Reason: To ensure adequate on-site parking provision in the interests of highway safety, convenience and amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.*

24. Prior to the commencement of the development hereby permitted details of secure and covered bicycle storage/ parking facilities for the occupants of the development shall be submitted to and approved in writing by the local planning authority. The cycle storage/ parking shall be implemented in accordance with such details as may be approved before occupation of the development hereby permitted, and shall be permanently retained in the approved form for the parking of bicycles and used for no other purpose.

*Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 4 (Sustainable Transport) and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.*

25. No part of any buildings hereby permitted shall be occupied until the visibility splays/zones indicated on the approved site layout drawing have been provided and maintained clear of any obstruction exceeding 0.6 metres in height at all times.

*Reason: In the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 & CP6.*

26. No development shall commence until details of the pedestrian/cycle accesses from the development site to Vauxhall Park and Viscount Way have been submitted to and approved in writing by the Local Planning Authority. The approved pedestrian/cycle accesses shall be permanently maintained, unless otherwise agreed in writing by the Local Planning Authority.

*Reason: To ensure that the site is permeable, to encourage use of the local park and to encourage the use of sustainable modes of travel. Relevant policy: NPPF and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.*

27. No development shall take place until full details of the Drainage System(s) have been submitted to and approved in writing by the Local Planning Authority. These shall include:

- a) Demonstration (through provision of calculation sheets) that peak discharge rates from the site to the local public sewer network will be no greater than those from the current site for the 1 year, 30 year, 100 year rainfall event, including the 100 year + CC event.
- b) Demonstration that all events up to and including the 1 in 100 year event including an allowance for climate change will be contained onsite and will not flood any of the proposed dwellings or neighbouring development.

- c) Demonstration of where and how surface water attenuation shall be provided across the site and that attenuation features are adequately sized to serve the development for all events up to the 1 in 100 year storm plus allowances for the effects of climate change.
- d) Demonstration that the design of the drainage system accounts for the likely impacts of climate change and changes in impermeable area, over the design life of the development.
- e) Soakaways and permeable paving (where proposed) will be designed in accordance with current guidance and as stated within The SUDS Manual CIRIA C697.
- f) Prior to detailed design of the soakaway structure, groundwater monitoring should be undertaken across the site to establish the depth of the seasonal groundwater table.
- g) Full details of all components of the proposed drainage system including source control, conveyance, storage, flow control and discharge. Details shall include dimension, locations, reference to storm simulation files, gradients, invert and cover levels and drawings as appropriate.
- h) Full details of the maintenance and/or adoption proposals/agreements for the development covering every aspect of the proposed drainage system.

*Reason: To prevent increased flood risk from surface water run-off.*

*Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10*

28. No development shall occur until an Employment Skills Plan has been submitted to the Local Planning Authority and approved in writing. The plan should demonstrate how the proposal will provide and secure opportunities for training, apprenticeship or other vocational initiatives to develop local employability skills required by developers, contractors or end users of the proposal. Once agreed the plan should be implemented in full in accordance with the agreed details.

*Reasons: In the interest of securing sustainable local employment. The development is in a countryside location and its benefits to local employment help justify the development.*

*Policies: NPPF, CP15, MDD Policy TB12:*

29. In the event that vegetation clearance is not possible during the winter period, October to February inclusive, in order to avoid breeding bird season then the following procedure will be followed:

- a) No more than 48 hours prior to vegetation removal a suitably experienced ecologist shall check the vegetation to confirm the absence of nesting birds.
- b) Where nesting birds are present in vegetation scheduled for removal the work must be rescheduled and active nests shall be appropriately protected until breeding is finished.

*Reason: To ensure that nesting birds, protected under the Wildlife and Countryside Act 1981 (as amended), are not adversely impacted upon as a result of the development.*

30. No development shall commence until details of the measures to be incorporated

into the development to demonstrate how 'Secured by Design (SBD)' accreditation will be achieved have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details, and shall not be occupied or used until written confirmation of the SBD accreditation has been submitted to the Local Authority.

*Reason: In the interests of the safety, crime prevention and amenity of future occupiers of the development.*

*Relevant policies: Paragraphs 58 & 69 of the National Planning Policy Framework and Core Strategy policy CP1.*

**Informatives:**

1. The development accords with the policies contained within the adopted / development plan and there are no material considerations that warrant a different decision being taken

2. The applicant is reminded that this approval is granted subject to conditions which must be complied with prior to the development starting on site. Commencement of the development without complying with the pre-commencement requirements may be outside the terms of this permission and liable to enforcement action. The information required should be formally submitted to the Council for consideration with the relevant fee. Once the details have been approved in writing the development should be carried out only in accordance with those details. If this is not clear please contact the case officer to discuss.

3. The applicant is reminded that should there be any change from the approved drawings during the build of the development this may require a fresh planning application if the changes differ materially from the approved details. Non-material changes may be formalised by way of an application under s.96A Town and Country Planning Act 1990.

4. This permission should be read in conjunction with the legal agreement under Section 106 of the Town and Country Planning Act the contents of which relate to this development.

5. The developer's attention is drawn to the fact that this permission does not authorise the physical construction of the proposed off-site highway works and site access connections to the public highway. A separate legal agreement (Minor Works Agreement) made with the Council under s.184/278 of the Highways Act 1980 is required. No work within or affecting the public highway shall commence until the agreement has been completed and the Council, as local highway authority, has approved all construction and installation details together including with a programme of works.

6. If it is the developer's intention to request the Council, as local highway authority, to adopt the proposed access roads etc. as highway maintainable at public expense, then full engineering details must be agreed with the Highway Authority at the Council Offices, Shute End, Wokingham. The developer is strongly advised not to commence development until such details have been approved in writing and a legal agreement is made with the Council under Section 38 of the Highways Act 1980.

7. Any works/ events carried out by or on behalf of the developer affecting either a public highway or a prospectively maintainable highway (as defined under s.87 New Roads and Street Works Act 1991 (NRSWA)), shall be co-ordinated and licensed as required under NRSWA and the Traffic Management Act 2004 in order to minimise disruption to both pedestrian and vehicular users of the highway.

Any such works or events, and particularly those involving the connection of any utility to the site must be co-ordinated by the developer in liaison with the Borough's Street Works team (0118 974 6302). This must take place AT LEAST three months in advance of the intended works to ensure effective co-ordination with other works so as to minimise disruption.

8. The applicant is advised that application site red edging incorporates land which forms part of the publicly maintained highway alongside the north section of the Loddon Bridge Road frontage. If it is the applicant's intention to enclose any part of this land, a Stopping Up Order using the provisions of Section 247 of the Town and Country Planning Act 1990 will be required

9. Thames Water wishes to inform the applicant that it will aim to provide customers with a minimum pressure of 10m head (approx. 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.

10. There is no right of discharge of highway drainage into the public sewerage system. An agreement to allow a discharge may be granted under section 115 (WIA 1991) by negotiation between the Highway Authority and Thames Water.

11. There is a requirement for the applicant to provide suitable private fire hydrant(s), or other suitable emergency water supplies to meet Royal Berkshire Fire & Rescue Service requirements.

#### **PLANNING HISTORY**

Numerous historic applications relating to the former commercial use of site.

The following have been determined more recently:

DEM/2011/1576 - Application for proposed demolition of 2 storey factory buildings silos and associated structures - Approval

F/2014/1107 - Proposed erection of 73 dwellings with associated roads, parking, amenity space, landscaping and creation of new access onto Loddon Bridge Road – Withdrawn

#### **SUMMARY INFORMATION**

Site Area	2.1 hectares
Previous land use	Commercial
Proposed units	68
Proposed density - dwellings/hectare	32.4 dph
Number of affordable units proposed	20
Parking: Total allocated spaces	124
Parking: Total garages	43
Parking: Total visitor spaces	28

<b>CONSULTATION RESPONSES</b>	
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Highways Authority	No objection, subject to conditions (Conditions 21 - 25).
Trees and Landscape	No objection, subject to conditions (Conditions 5 - 7).
Environmental Health	No objection, subject to conditions (Conditions 8 - 13).
Affordable Housing	No objection raised. Provided advice on the required level of affordable housing provision required at the site.
Ecology	No objection, subject to condition (Condition 29).
WBC Drainage Officer	No objection, subject to condition (Condition 27).
Environment Agency	Provide standing advice.
Thames Water	No objection, subject to condition (Condition 27).
Education Services	No objection, subject to appropriate infrastructure contributions.
Berkshire Archaeology	No objection, subject to condition (Condition 20).
Crime Prevention & Design Advisor	No objection, subject to condition (Condition 30).
Thames Valley Police	Request contributions to cover impact on police infrastructure
Royal Berkshire Fire & Rescue:	No objection, subject to informative

<b>REPRESENTATIONS</b>	
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Rob Wilson MP	<p>Residents not against development itself, but are concerned about planned access onto Loddon Bridge Rd – safety fears and increased traffic</p> <p>Sensible solution to distribute traffic over two entrances, as discussed in July public meeting involving Rob Wilson MP.</p> <p>Whilst traffic surveys may suggest that one access point is a feasible option, those living on Loddon Bridge Road disagree.</p> <p>Any decision should deliberate the vital importance of alleviating congestion and avoiding potential accidents.</p>
Winnersh Parish Council	<p>The Committee considered the proposals and recommended the application be refused on the following grounds:</p> <ul style="list-style-type: none"> <li>- There are safety concerns regarding the siting of the access on Loddon Bridge Road. Residents of Loddon Bridge Road in the vicinity of the proposed entrance to the site already find it very difficult and dangerous to exit their driveways due to the amount of traffic and poor visibility caused by a bend in the road. This will also apply to people exiting the proposed development. The Committee did not think it appropriate for there to be any access onto Loddon Bridge Road and felt strongly that the access to the development should be from Viscount Way.</li> <li>- Many school children use this area of Loddon Bridge Road, on</li> </ul>

	<p>bicycles and on foot, and their safety will be put at risk by traffic using the proposed site entrance.</p> <ul style="list-style-type: none"> <li>- As the proposed entrance is next to a blind bend in Loddon Bridge Road, traffic exiting the site will find it difficult to turn right, particularly during rush hours and most will therefore turn left. Much of this traffic will then turn into Vauxhall Drive to avoid the bottleneck at the southern end of Loddon Bridge Road, thereby exacerbating the traffic problems in Vauxhall Drive.</li> <li>- The Wokingham Borough Managing Development delivery document (Local Plan), adopted 21 February 2014, identified this site as appropriate for the delivery of around 57 dwellings. This proposal is for 68 dwellings.</li> <li>- There is insufficient parking provided on the site.</li> <li>- There appears to be footpath access to the rear of some properties, which does not appear to be in compliance with Safer Homes Initiatives.</li> <li>- The acoustic fence to the north of the site does not appear to be appropriate for the nature of the business carried out in the industrial estate. The proposed acoustic fence is 2m high, whereas the HGV vehicles using the industrial estate are 4m high.</li> <li>- The proposed 2m fence to the north of the site will not provide sufficient protection to the industrial units, where high value goods are sometimes stored.</li> <li>- The noise impact assessment gives the measured noise levels from the 24 hours a day truck movements on the industrial estate, and general traffic noise, and states that in order for acceptable internal levels of noise to be achieved the windows of the dwellings adjacent to the industrial estate must remain closed day and night. This is not acceptable.</li> </ul> <p>If permission is granted for this development the Committee request that the following concerns be addressed:</p> <ul style="list-style-type: none"> <li>- High level windows in the industrial units to the north of the site will overlook the adjacent houses and gardens.</li> <li>- CCTV cameras covering the industrial estate will overlook the adjacent houses and gardens.</li> <li>- The HGV vehicles using the industrial estate have cabs at a height of 3m, which will overlook the adjacent houses and gardens.</li> <li>- The existing business operating to the north of the site has to renew its 24 hour HGV operator's licence every 5 years and should be protected against objections by the new residents in order to comply with the Wokingham Borough Managing Development Delivery Document (Local Plan), which for this site specifically states that "the proposals must deliver appropriate measures to safeguard the amenities of the occupants of the dwellings whilst ensuring continued B class activities can continue within adjoining Headley Road Core Employment Area"</li> <li>- The boundary treatment to the south of the site must provide protection to the historic bridle way in this area</li> </ul>
Ward Member(s)	<b>Cllr Baker</b> commented that he had concerns about the proposed new access onto Loddon Bridge Road

<p>Neighbour Comments</p>	<p><b>20 letters of objection and 5 letters of comment</b> have been received from residents, raising the following issues:</p> <p><b>Highway safety</b></p> <ul style="list-style-type: none"> <li>- Objections to the new single access being created onto Loddon Bridge Rd – effect of potentially 140 additional cars</li> <li>- Some residents would accept access onto Loddon Bridge Rd if there was a second access onto Viscount Way.</li> <li>- Others object to ANY access onto Loddon Bridge Rd irrespective of a second access. They suggest Viscount Way not as populated and traffic flow can be managed via roundabouts or traffic light systems.</li> <li>- Developers should purchase strip of land in order to enable Viscount Way access.</li> <li>- Loddon Bridge Rd safety issues highlighted e.g. bend in road and visibility issues of existing properties entering/exiting driveways</li> <li>- History of accidents.</li> <li>- Bad traffic jams along Loddon Bridge Rd at peak times (also bottle necks along Bader Way and Headley Rd).</li> <li>- Knock on traffic impacts on surrounding roads such as Vauxhall Drive and Fosters Lane.</li> <li>- No crossing points along this section of Loddon Bridge Rd</li> <li>- Likely to be on-street parking problems, as not enough parking within development.</li> <li>- Cumulative effect of various developments in Woodley</li> <li>- Previous access onto Loddon Bridge Rd was closed and switched to Viscount Way for safety reasons. Since then, traffic has increased due to numerous developments</li> <li>- Four dwelling scheme refused opposite proposed access on highway safety grounds (this scheme was on inward bend of LBR, compared to proposed access)</li> <li>- Many residents comment that a school is located nearby (Rivermead School). Main commuter route for other schools.</li> <li>- Inconsistencies in transport assessment (timing of measurements) and data challenged (e.g. maximum of one car queuing to get out of Fosters Lane).</li> </ul> <p><b>Services and facilities</b></p> <ul style="list-style-type: none"> <li>- Cumulative effect of various developments in Woodley</li> <li>- Schools, doctors surgeries and dentists already under pressure, and this would be worsened.</li> <li>- No extra shop units proposed.</li> <li>- Plans do not propose additional parking at Woodley Precinct which is already at capacity</li> <li>- Woodley has more housing than amenities, especially for youth.</li> </ul> <p><b>Trees and biodiversity</b></p> <ul style="list-style-type: none"> <li>- Concern about removal of trees adjacent to bridle path shown in Tree Survey section 5 (Tree Officer confirms although</li> </ul>
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	<p>several are category C trees, they are to be retained)</p> <ul style="list-style-type: none"> <li>- Air and noise pollution.</li> <li>- Scots Pines at the rear of No 23 Vauxhall Drive in danger of removal and should be TPO'd (no plans for removal, tree protection shown).</li> </ul> <p><b>Boundaries</b></p> <ul style="list-style-type: none"> <li>- Boundary treatments: state of existing chain link and concrete fence between bridle way and development needs addressing, no indication of boundary treatment (conditions stage).</li> </ul> <p><b>Relationship with neighbours</b></p> <ul style="list-style-type: none"> <li>- An objection from the business backing onto the proposed development (Headley Park Area 8). This is a 24hr 7 days a week transport and warehouse operation which has no restrictions. Trucks and fork lifts have audible reversing beepers and question accuracy of Noise Survey.</li> <li>- This could affect the enjoyment of the proposed properties, and lead to time operating restrictions which would be unfair given that the commercial use was in place first, placing 65 jobs at risk.</li> <li>- The acoustic fence to the north of the site does not appear to be appropriate for the nature of the business carried out in the industrial estate. Inadequate to maintain privacy.</li> <li>- 24 hours a day truck movements on the industrial estate, and general traffic noise, will be a disturbance to new residential properties.</li> </ul>
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## **PLANNING POLICY**

### National policies:

National Planning Policy Framework (NPPF)

National Planning Policy (NPPG)

### Wokingham Borough Local Development Framework – Core Strategy:

CP1 – Sustainable development

CP2 – Inclusive communities

CP3 – General Principles for Development

CP4 – Infrastructure Requirements

CP5 – Housing mix, density and affordability

CP6 – Managing Travel Demand

CP7 – Biodiversity

CP8 – Thames Basin Heath Special Protection Area

CP9 – Scale and location of development proposals

CP10 – Improvements to Strategic Transport Network

CP17 – Housing delivery

Managing Development Delivery Local Plan Document:

*Cross Cutting Policies:*

- CC01 – Presumption in favour of sustainable development
- CC02 – Development Limits
- CC03 – Green Infrastructure, Trees and Landscaping
- CC04 – Sustainable Design and construction
- CC05 – Renewable energy and decentralised energy networks
- CC06 – Noise
- CC07 – Parking
- CC09 – Development and Flood Risk
- CC10 – Sustainable Drainage

*Topic Based Policies:*

- TB05 – Housing Mix
- TB07 – Internal Space Standards
- TB09 – Residential accommodation for vulnerable groups
- TB21 – Landscape Character
- TB23 – Biodiversity and development
- SAL02 – Allocated housing development sites

Other guidance:

- Borough Design Guide SPD
- Sustainable Design and Construction SPD
- Planning Advice Note (Infrastructure Impact Mitigation Contributions for New Development – Revised March 2014)
- Affordable Housing SPD

**PLANNING ISSUES**

**APPRAISAL**

**Principle of Development:**

1. The site is located within the settlement boundary of Woodley, a major development location, and as such development of new residential dwellings is acceptable in principle providing that its detail complies with the principles stated in the Development Plan.
2. The site was excluded from the Core Employment Area and allocated for residential development in the Managing Development Delivery Development Local Plan (MDD). Policy SAL02 states that it should only be used for this purpose. It designated the site

(WD110) for development of 'around 57' dwellings (policy SAL02). The MDD was adopted in February 2014 and as such the principle of development on the site is acceptable.

3. MDD Appendix 12 includes specific requirements for each of the allocated sites. The following requirements are set out for this site:

- a) Delivery of around 57 dwellings;
- b) Subject to viability in line with Core Strategy Policy CP5, provision of at least 30% of affordable dwellings;
- c) Deliver appropriate measures to safeguard the amenities of the occupants of the dwellings whilst ensuring continued B class activities can continue within adjoining Headley Road Core Employment Area; and
- d) Information is supplied with the application demonstrating how it addresses any impacts on the environment and landscape of the area.

4. The NPPF requires that proposed dwellings should be well integrated with, and complement local buildings in relation to scale, density, layout and access. Policy CP3 of the Core Strategy outlines that development should be appropriate to the surrounding streetscene, and without detriment to the amenity of neighbouring residents. The current position with regards planning policy encourages character led development. The Borough Design guide provides substantial advice on layout best practice.

#### **Impact on the character of the area:**

##### Density:

5. The site has been identified for an allocation of 'around 57 dwellings'. The site area is 2.1ha, so the proposal for 68 dwellings equates to 32.4 d/ha.

6. The site is approximately 200m from another allocated site (Former Linpac site). The western section of this site was developed at 40 d/ha (O/2013/0668 – 34 dwellings), although the majority of the site is a lower density of 25 d/ha (F/2013/1136 – 93 dwellings). The difference between the proposals and this scheme is in part due to the predominantly detached larger dwellings on that site, compared to the significant number of semi-detached properties proposed in this scheme. Whilst the density is higher than surrounding 1950/60's housing to the west and south, it is noted that housing in this period was typically built with much lower densities.

7. Whilst minimum densities have been removed by previous planning policy, the NPPF still encourages the efficient use of land. The development is located on a sustainable brownfield site and the proposed density is considered appropriate to its suburban context, and would not appear unduly excessive in the context of other developments in the Borough (SDL's being built to an average density of between 30-35 d/ha).

##### Housing Mix:

8. The proposal for 68 new dwellings incorporates the following mix:

- 3 x 1 bed flats
- 6 x 2 bed flats
- 7 x 2 bed houses
- 33 x 3 bed houses
- 18 x 4 bed houses

- 1 x 5 bed houses

9. This incorporates several different housing types to cater for a range of household sizes. This is considered to be an acceptable mix to meet the requirements of the Core Strategy and MDD. The Affordable Housing Officer is satisfied that the mix allows for potentially adequate affordable housing to be provided to meet local need and as such this is acceptable.

Scale, design and layout:

10. A new access is proposed from Loddon Bridge Road, with the development arranged around a loop road system. The dwellings will be set back from Loddon Bridge Road with the existing mature tree boundary belt along this boundary retained, creating strong screening to the site from Loddon Bridge Road. This is a key component of the scheme.

11. The majority of dwellings are two storey, however focal points are provided within the site from 2.5 and 3 storey buildings. A 3 storey terrace is proposed where the access road splays into two roads, creating a gateway building and framing the informal housing square. The square is framed by buildings and has two exits leading to streets of detached and semi-detached houses. Further east these streets take on the characteristics of a mews area.



12. The 3 storey apartment in the north-east corner frames the proposed eastern pedestrian access into the site and gives the development frontage onto Viscount Way. The east of the site is bordered by Viscount Way and open parkland, allowing an opportunity for a 2.5 storey building to front this space. The dwelling frontages are well set back from the eastern boundary, allowing for landscaping to be introduced along this boundary. A new pedestrian link between the development and public open space to the east will be created.

13. A key consideration for the layout is the presence of commercial units to the north, and dwellings have been proposed to back onto the northern boundary, where a 3m high acoustic fence will be provided.

14. The dwellings are set back from the southern boundary, with the gardens backing onto this boundary along with the existing trees and vegetation to be retained creating a green buffer between the new development, public footpath and existing residential properties to the south.

15. The road layout makes the most efficient use of the site, with built form providing a continuous frontage along the proposed streets. Indeed Secure by Design principles have been incorporated into the scheme, ensuring that all public and parking areas benefit from good natural surveillance.

16. The street elevations have been designed to create interest and visual variety within an overall architectural theme reflecting some of the 20<sup>th</sup> century traditional house styles along Loddon Bridge Road. The key materials will be brickwork for the walls with occasional areas of render and contrasting bricks used for details such as window headers or cills. Roofs will be finished in clay tiles and slates. Features such as hipped roofs, gables, bay windows, porches and chimneys are proposed.

17. The Affordable Housing SDP states that the aim is to create development which is 'tenure blind' to meet the policy requirements of high quality design and sustainable mixed communities. The affordable units have a similar overall design to the rest of the development which is considered acceptable. They are reasonably mixed throughout the site, featuring along the northern and southern boundaries, and are grouped in clusters for ease of maintenance, meeting the requirement of social housing providers.

18. Following a reduction in the number of dwellings proposed, the Tree & Landscape Officer is satisfied that the site is capable of accommodating the development proposed whilst delivering a well landscaped form of development. The revisions now enable sufficient vertical green infrastructure along the street scene and some opportunities to plant large trees within the site. The tree cover on the southern loop road has improved in particular. In several cases, proposed trees have been moved from locations within rear gardens to street locations, so that they are less liable to removal by residents. Subject to conditions relating to landscaping details, a landscape management plan and tree protection details the development is considered acceptable in tree and landscape terms.

#### *Amenity space & Internal Space Standards:*

19. The Borough Design Guide establishes requirements for private amenity space (Section R16). Amenity space for houses should generally have a 'roughly rectangular shape' and good access to sunlight, with a useable private garden of at least 11 metres in depth.

20. Revised plans were received removing two dwellings from the scheme, enabling all but three gardens to now meet the 11m requirements. Reducing the number of dwellings within the central block enabled plots to be moved further from the southern boundary to provide garden depths that exceeded the 11m requirement, considered necessary given the maturity of boundary vegetation.

21. The three plots which fall short of the 11m depth requirement, have the following additional width:

Plot No.	Garden Length.	Garden Width.
25	9.1m.	12.3m.
30	8.5m.	12.8m.
61	10.1m.	17.0m.

22. Given that these are much wider than average and as corner plots have more open aspect these are all considered to be useable and acceptable in this instance.

23. The proposed flats (plots 14-22) have access to a communal garden. At the conditions stage this should be landscaped to provide privacy from the parking area (hedge), with tree planting along the northern boundary to provide a high quality useable space.

24. Therefore the proposed layout succeeds in achieving roughly rectangular gardens and of at least the minimum recommended 11m depth with all plots except three corner plots. All gardens are considered to be of a usable nature compliant with the aims of the Borough Design Guide.

25. The MDD main modification version policy TB07 seeks to achieve certain internal space standards in new dwellings. These have been achieved on all dwellings within the site, including the affordable housing units.

### **Residential amenities:**

26. The existing site is generally level and the majority of dwellings proposed are 2 storey. The site was formerly a bakery, and changes of level within the site are largely due to the footprint of former buildings which have now been demolished. The Borough Design Guide sets out minimum recommended separation distances between dwellings in order to preserve amenity and character. The recommended distances are as follows:

27. Overlooking and Privacy depth distances are set out in the Design Guide and are as follows:-

#### 1-2 Storeys:

Front to front elevation	10m
Back to back elevation	22m
Back to flank elevation	12m

#### Over 2 storeys:

Flank to boundary	2m
Front to front elevation	15m
Back to back elevation (houses to houses)	26m
Back to back elevation (Upper floor living room)	30m

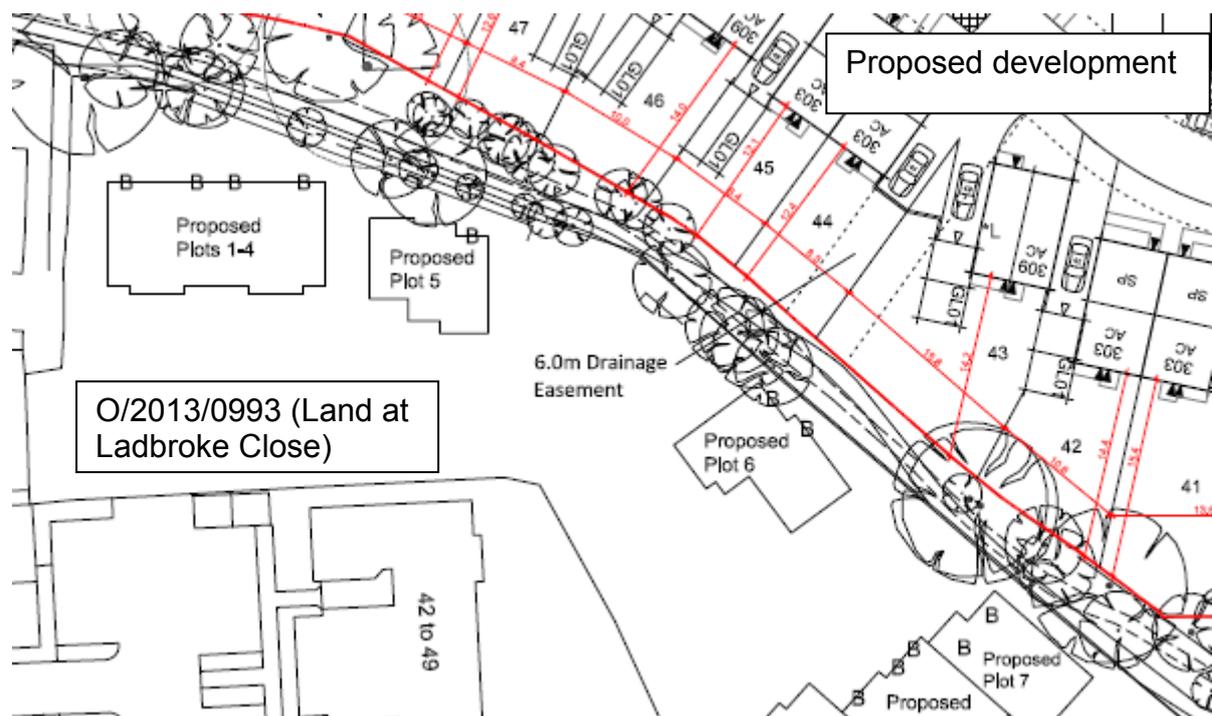
28. The site is bounded by Headley Road East Industrial Estate to the north, a vehicle rental storage yard and parkland to the east, residential properties to the south (with a bridleway separating) and Loddon Bridge Road to west (with a good separation to

residential beyond).

29. Therefore the key boundaries of the development site are the northern and southern boundaries, which will now be explored further.

#### Relationship with residential properties south of site

30. The following properties are located to the south of the development; No 284 Loddon Bridge Rd, No's 5–14 Ladbroke Close, plots 1–7 of approved application O/2013/0993 (layout was agreed at outline stage), and No 23-31 Vauxhall Drive. It is worth noting that mature landscaping, as well as a bridleway, separate these properties from the development



31. In all cases, 2 storey dwellings proposed would be more than 22m from residential dwellings and 3 storey dwellings (Plots 36, 40) would be more than 26m from residential dwellings to the south of the site.

32. The only exception to this relates to plots 6 & 7 of O/2013/0993. These relationships were improved in revised plans, which increased the boundary separation and the orientation (to make oblique) of plots 41, 42 & 43, in response to the close proximity of plots 6 & 7 Ladbroke Close to the boundary. The development site is approx 1m higher than Ladbroke Close.

33. The revised plans show a separation distance of 18m between plot 6 Ladbroke Close (bedroom window) and plots 43 & 44 of the development, and a separation of 20m between plot 7 Ladbroke Close (stairwell window) and plot 42 of the development. These distances whilst not meeting the 22m, are considered acceptable given the oblique angle between properties. It is also noted that plots 6 & 7 of O/2013/0993 have not been constructed and only have 'outline' consent.

34. In addition to the above considered distances, it is intended that tree protection fencing will protect existing boundary tree screening. To supplement this it is suggested

that additional landscaping is planted, in particular between plots 41-48 where it is more sparse. The Crime Prevention & Design Advisor has recommended 1.8m hit and miss fencing with trellis or 2.0m open topped metal railings for properties along this boundary (with a planting buffer). This is to provide an opportunity for natural surveillance over the public footpath, making it safer for users, whilst providing security and privacy to the dwellings. A boundary and landscaping scheme at the conditions stage will need to address all of the above considerations.

35. It is considered that all proposed dwellings will have acceptable relationships with existing properties to the south of the site, such that no significant loss of light, overbearing or overlooking impacts will result.

#### *Relationship with commercial uses north of site*

36. Commercial uses are located to the north of the site. The overlooking impact of first floor windows in the BDT building has been raised. There are three south facing windows in the eastern wing which have a back-to-back relationship with proposed northerly plots, and an east facing window which has an oblique relationship. There is no separation guidance for residential to commercial uses. However even taking the BDG's residential guidance, the first floor commercial windows have a 28m back-to-back separation which is considered sufficient to avoid overlooking.

37. CCTV is located on each corner of the BDT building and within the site, and the potential privacy impact of this has been raised in relation to northerly plots. At its closest, CCTV is located 1m from plot 1's boundary at a height of approx. 5.5m. It is also noted that lorries using the adjacent yards will have a cab height of 3m, however this is less of a concern with the acoustic fence height being proposed at 3m.

38. It would be desirable for boundary landscaping to grow above the height of the fence to provide an additional visual screen. One option is to plant pleached hornbeam trees, given that they provide all year screening (evergreen) and minimise the loss of gardens, given their small canopy areas. They would also grow to the required height to ensure privacy from CCTV cameras, in particular to plots 1-4. The Environmental Health Officer has confirmed that the pleached trees would not harm the integrity of the fence or lead to maintenance issues. Another option is to design a trellis above the fence and to plant climbers (ivy, clematis). There are a few workable solutions which will provide the necessary mitigation and further details will need to be provided at the conditions stage. It is therefore considered that privacy can be safeguarded to these properties.

39. The proposed 3m high acoustic fence is not considered to have an overbearing/loss of light impact on adjacent plots. A 3m high acoustic fence was found to have an acceptable relationship with properties at the nearby Linpac redevelopment, and planting can help soften its appearance. It should be noted that some of the proposed dwellings at the Linpac site were closer to the acoustic fence than those in the current proposal, but this was still considered acceptable by the appeal inspector.

#### *Relationships within the development site*

40. The scheme has been designed to comply with the Borough's separation distances and there are no areas of concern with regards poor amenity between individual plots.

41. The majority of dwellings proposed are 2 storey, although there are instances of 2.5

and 3 storey dwellings which have the potential for greater impacts. Plots 53-55 (3 storey terrace) have a 17m side-to-rear relationship with plot 56. For clarity, the second storey rear facing windows will be conditioned to be obscure glazed as they serve en-suites. A note on the site layout states that there will be obscure glazing to all bathrooms & toilets, plus obscure glazing on certain side windows where shown, i.e. plots 31 & 43 (to a landing window) and plot 64 (to a secondary bedroom window). The 3 storey apartment block (plots 14-22) has an acceptable relationship with surrounding neighbours.

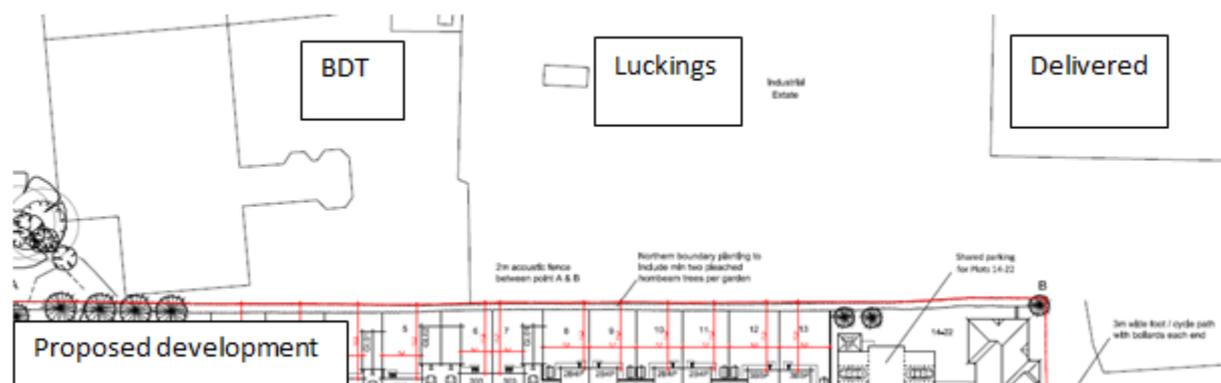
42. Note separation was increased between plots 26-8 and 33-5 in revised plans to comply with 22m back-to-back distance. Velux windows in the roofspace of plots 33-5 are not considered to lead to any direct overlooking given their orientation, and therefore this revised separation is considered acceptable.

43. It is considered that no unacceptable loss of light, overbearing or overlooking impacts arise as a result of this proposal.

### **Noise and Disturbance:**

44. The application includes a noise assessment that considers noise from HGV movements (from north and north east), and traffic noise from the Loddon Bridge Road (to west).

45. Three haulage firms abut the northern boundary of the development (BDT, Luckings and Delivered). They all have unrestricted 24hr/7day-a-week operations. For each business, lorries are required to have a 15 minute check before being operated (by law). This includes use of reversing beepers and indicators. Lorries use a one way system, exiting the site along the right of way adjacent to the developments northern boundary and onto Viscount Way.



46. The main operations of BDT take place between 6am-8pm. However it is not unusual for 4/5 lorries (from a fleet of 20) to drive to a delivery during the night. Their loading bays are located a good distance from the development and not commonly used at night. There is a similar set up at Luckings, although night time loading/unloading is more common and located closer to the development boundary. Potential future intensification of the businesses must be recognised.

47. This means that the proposed development will be subject to some noise at anti-social hours, in particular from loading/unloading, vehicles using reversing beepers, and vehicles exiting the site adjacent to the shared boundary (although it is noted that lorry

engines are located below the cabin).

48. MDD Policy CC06 (Noise) states that proposals must demonstrate how they have addressed noise impacts (a Noise Impact Assessment has been submitted). Where there is an adverse noise effect, which is not mitigated by review of the development layout or internal layout, the next step is physical mitigation measures such as barriers/mechanical ventilation.

49. The development layout has been designed to maximise the separation distance to the commercial uses (properties backing onto rather than flanking northern boundary) and the internal layout of properties has been designed to minimise bedrooms adjacent to the HGV route where possible. However this alone is insufficient, and as required by Policy CC06, the Noise Impact Assessment identifies physical mitigation measures:

- An acoustic fence running along the length of the northern boundary (marked A to B on the site plan).
- High specification double glazing (glass thickness and cavity depth vary as necessary across the site)
- Ventilation system (there is a reliance on windows being shut for acceptable noise levels to habitable rooms, and therefore ventilation will be provided through a mechanical ventilator as alternative to open windows).

50. Concerns were raised leading to submission of a supplementary Noise Impact Assessment. This followed a meeting with the developer and Environmental Health Officer (EHO), to discuss concerns raised by a neighbouring business. These concerns were the accuracy of the noise assessment data in relation to DBT's operation, and the fact that it didn't account for the new business 'Delivered', as well as consideration for potential increases in the commercial activity as businesses expand.

51. The EHO has the following comments about the supplementary Noise Impact Assessment:

- Additional noise sources and locations have been taken into account with an increased amount of night time activity included in the revised modelling.
- This has led to the applicant proposing a 3m rather than 2m high acoustic fence along the northern boundary of the site
- The noise mitigation zones which dictate the specification of double glazing at various locations have been modified according to the revised noise data.
- As a result, a higher category glazing for houses along the northern boundary is proposed (10mm thick glass rather than 4mm thick glass for bedrooms).

52. There is still a reliance on windows being shut within the development for acceptable noise levels, however this was an accepted solution at the nearby Linpac redevelopment, which had a similar residential/commercial relationship.

53. The EHO has reviewed the supplementary Noise Impact Assessment and considers that the revised mitigation measures will ensure an acceptable noise environment for the proposed residential development. This is subject to conditions relating to submission of a detailed glazing strategy and installation, retention and on-going maintenance of the acoustic fence.

54. The revised mitigation measures ensure an acceptable noise environment for the proposed residential development now and also build in safeguards for the future should activity on the adjoining sites change in nature or increase in activity. This will

ensure not only that residential properties are not detrimentally affected now or in the future, but will ensure that commercial businesses adjoining the site can remain and maintain viable businesses.

55. Finally, a BMX facility is located adjacent to the development site at the western end of Vauxhall Park. This is a fairly small facility, although it is acknowledged that its use could increase as a result of the development. The development has been designed to have a verge with landscaping to abut the facility, and given its scale is not considered to have a harmful impact on nearby plots.

### **Contamination:**

56. The proposed use is sensitive in nature being residential with private gardens. The site has been previously occupied by a large bakery which has now been demolished. This land use has the potential to give rise to contamination in near surface soils at the site. In accordance with the National Planning Policy Framework 2012 and the principles of sustainable development an assessment of potential contamination is required to determine if the site is suitable for the proposed end use.

57. A Desk Study and Ground Investigation Report for the former Allied Bakeries Site dated April 2013 by Hydrock (Ref: R/13089/001) has been submitted with the application. This provides details on an acceptable Phase 1 preliminary risk assessment and Phase 2 intrusive investigation. Contamination has been identified on site which could present an unacceptable risk to end users and controlled waters. This does not preclude the proposed residential development but the Environmental Health Officer recommends conditions to ensure remediation is carried out and the site made suitable for use.

### **Access and Movement:**

#### **Highway Safety & Road Design**

58. The previous bakery use of the site ceased in 2006. The bakery had a single access onto Viscount Way. It is proposed to switch the single access onto Loddon Bridge Road to serve the residential development, to avoid potential conflicts between residential and commercial vehicles. There is also the issue of gates along Viscount Way being locked when the neighbouring commercial premises are closed.

59. It is proposed for the residential development to have a single access via a new simple priority junction onto Loddon Bridge Road. Loddon Bridge Road is subject to a 30mph speed limit, has footways and street lighting. The proposed junction has been designed with suitable geometry and visibility which accords with the Department for Transport publication Manual for Streets.

60. Representations have been made regarding possible conflict with vehicles using the proposed junction and vehicles entering and exiting crossover accesses to houses on the opposite side of Loddon Bridge Road. Conflicting vehicle movements will be minimal and will have a negligible effect on the operation and safety of the proposed junction and the existing accesses. A Stage 1 Road Safety Audit has been carried out for the proposed access arrangement and other than a requirement to provide and

maintain visibility splays no concern has been raised.

61. Whilst it is proposed to close the existing vehicle access from the application site to Viscount Way, a footpath/cycle link to this road will be provided. Viscount Way is an unadopted private road at this location, and whilst access has been gained along this route in the past it is not clear from the application documents that this right will exist in the future. A footpath/cycle link from the application site to the adjoining recreation ground is also shown which will improve the permeability of the site for pedestrians and cyclists.

62. The estate road layout incorporates the design principles of *Manual for Streets* and the council's Highway Design Guide. The internal road layout is in part designed with footways and in part designed as shared surfaces, which is acceptable for the number of dwellings proposed. Revised plans show visibility splays for the central section of housing which has a reduced service margin. The Highways Authority accepts, given the intended design speed, the proposed 2m x 15m vision splays. With the service margin only 1m within this area, the proposed splays would cross over future conveyed land, and a condition will be used to ensure these are kept free of obstruction. The Highways Officer raises no objection to the swept path analysis as shown on the revised plan.

#### Traffic Impact

63. The planning application is supported with a Transport Assessment (TA) produced by WSP. The TA provides an estimate of likely traffic generation of a 70 dwelling scheme in comparison with the historical use of the site when operating as B2/B8 commercial uses. The trip rates used have been derived from the TRICS database. The AM peak hour trip rate when averaged between the mix of flats and houses is similar to the trip rates used in the Wokingham Strategic Transport Model. However, the PM peak hour trip rate is noticeably higher. The data from TRICS is derived from a single day survey and may not be representative of traffic generation for housing in this area. Nevertheless, given that the estimated traffic generation is higher than expected, the Highways Officer does not consider that the assessment needs to be amended.

64. Although the Highways Officer has not agreed with the calculated traffic generation of the proposal, it is important to note that the application site historically has operated with B2/B8 commercial uses and the Highways Officer does accept that the net change in traffic movements onto the highway network will not be materially different. The pattern will however be reversed and traffic will be reassigned directly to Loddon Bridge Road rather than via Viscount Way/Spitfire Way/Miles Way which will alter localised impacts.

65. The Transport Assessment (TA) has included information on personal injury accidents in the vicinity of the application on Loddon Bridge Road. The nature and pattern of the recorded accidents does not suggest an inherent defect with the design of the adjoining highway network and the Highways Officer therefore considers that the additional development traffic can be safely accommodated at this point.

#### Parking

66. The council's Parking Standards Study Report provides guidance on the likely parking demand associated with new development, with the calculated provision being

dependent on the size and type of dwelling and the allocation of spaces. The parking provision and allocation is illustrated on drawing no. SO-003-SL-101 entitled Parking Strategy. The parking provision consists of 124 allocated parking spaces and 43 garages and 28 unassigned/visitor spaces.

67. The amount of allocated parking is generally proportionate to the size of dwelling. All single garages are to be a minimum of 3m by 6m in length in line with the council's guidance.

68. The scheme provides for 28 unassigned/visitor spaces; this number is broadly in line with council's parking standards. The spaces have been distributed evenly throughout the road layout with the majority being incorporated into areas that will be eligible for adoption as public highway. This approach provides the opportunity for greater flexibility and better use of the spaces.

### Sustainability

69. The Transport Assessment includes an evaluation of the accessibility of the application site by walking, cycling and public transport to local facilities. The application site is within convenient walking and cycling distance of many local facilities, education and employment areas. Public transport accessibility is satisfactory; Loddon Bridge Road/Vauxhall Drive is a bus route with stops located in reasonable distance to the site.

70. Secure/covered cycle parking should be provided for all units. Some garages are large enough to comfortably accommodate cycle storage and a parked car. Where garages are less than 3m by 7m additional sheds should be provided. The cycle store for the flats is shown to accommodate 10 cycles however the spaces are very constrained. Ideally 'Sheffield' type stands should be provided with a minimum of 800mm spacing, and this has been secured by condition.

71. Whilst the traffic generation from the proposed residential redevelopment will not materially alter traffic conditions on the highway network compared to when the previous commercial uses on the site were in operation, the proposal will result in a change in travel demand as the future residential occupiers of all ages will have different needs than an adult workforce previously employed at the site. The Transport Assessment has included a Travel Plan Statement which outlines the developer's intention to provide informative measures to promote sustainable travel choices. However, this application does not include any measures to improve pedestrian, cycling or public transport facilities/provision. The highway authority has sought Section 106 contributions from the developer, to help deliver appropriate transport infrastructure in-keeping with Wokingham's Core Strategy to promote/improve sustainable modes of travel.

72. Given the above considerations, the Highways Officer raises no objection to the proposed scheme.

### **Sustainable Design and Construction**

73. The application is accompanied by a Sustainability Statement (contained with DAS) and Energy Statement which demonstrates that energy saving and generating measures will be undertaken on site in accordance with the Sustainable Design and

Construction SPD.

74. MDD policy CC05 states that planning permission will only be granted for proposals that deliver a minimum 10% reduction in carbon emissions through renewable energy or low carbon technology where the development is for schemes of more than 10 dwellings. The energy statement states that the development will meet the reduction in emissions through the use of energy efficiency measures such as improved thermal fabric efficiency, air tightness and effective heating controls. Gas saver technology will be applied to boilers. These measures will be secured by condition.

75. The DAS indicates that the development will be designed to a minimum of Code Level 3 however the MDD policy CC04 seeks to secure Level 4. As no pre-assessment has been submitted, further details are required by condition. If it is intended that the houses be built to less than Code level 4, full details of why Code Level 4 is not achievable on site must be provided to the Local Authority at the conditions stage.

### **Flood risk, Drainage and Water:**

#### *Flood Zones and Flood risk*

76. The NPPF carries forward the sequential approach to flood risk, establishing the development should be directed away from the areas at highest risk and, where development is necessary in areas at risk of flooding, it should be made safe without increasing flood risk elsewhere. Core Strategy Policy CP1 and MDD submission policy CC09 are consistent with this approach, requiring that new development should avoid increasing and where possible reduce flood risk.

77. The EA have confirmed that the proposed development is located in Flood Zone 1, where the risk of flooding is low and as such all forms of development - including 'more vulnerable' uses - are acceptable. Therefore, the proposal complies with the sequential test with regards flooding.

#### *Drainage:*

78. Whilst the use proposed is appropriate for Flood Zone 1, it is important to ensure that adequate drainage exists at the site to ensure the appropriate handling of surface water both for this development and the surrounding properties.

79. Paragraph 103 (footnote 20) of the NPPF sets out that a Flood Risk Assessment (FRA) should be submitted for all developments over one hectare in size (this site is approximately 2 hectares).

80. An FRA has been submitted with this application. Following advice from the Councils Drainage Officer, this FRA was amended in November and December 2014 (Rev 4).

The latest amendment provided additional information in the following areas:

- Soakaway dimensions, contributing areas and supporting calculations;
- Infiltration rate justification including details of locations of soakage testing;
- Updated calculations for the proposed permeable paving; and,
- Identification of the volumes of surface water to be stored onsite, where these will be stored and the contributing area for each drainage component.

81. In general the Drainage Officer considers that the information available relating to flood risk of the site to be sufficient, and meets all of the requests that were issued in December 2014. It is also considered that sufficient information has been provided regarding the design of the soakaways, the calculations for permeable paving and justification of the infiltration rates used within the design.

82. Concerns remain regarding the design of the Indicative Surface Water Drainage Strategy. However it is considered that solutions to address this can be provided within the proposed development without impacting on the form of the development. On this basis the Drainage Officer and Thames Water recommend approval of the application subject to full details of the Drainage System being agreed at the conditions stage.

**Bridleway:**

83. Bridleway 7 runs along the southern boundary of the development site. The Public Rights of Way Officer (PROW) comments that no attempts have been made to directly link the development to this bridleway.

84. A link was considered however discounted for two reasons. Firstly, a link between houses would have been undesirable from a crime prevention design perspective. Secondly, there would be little benefit of a link onto the bridleway given that the alternative route to Vauxhall Drive via Vauxhall Park would have a similar walking distance (i.e. to the bus stop on Vauxhall Drive).

85. A landscaping condition is also recommended in respect of the southern boundary, and additional planting will have a knock-on effect of making the bridleway more attractive for users.

**Crime prevention:**

86. The Crime Prevention & Design Advisor notes that the Design and Access Statement has a specific section on Crime prevention (6.10). "*secured by design principles to be adopted across the site*". To ensure that opportunities to design out crime and/or the fear of crime and to promote community safety are not missed, it is recommended that a condition requiring construction to "Secure By Design" accredited standard be imposed.

**Archaeology:**

87. An archaeological desk-based assessment has been submitted in support of this application (Thames Valley Archaeological Services, report 13/202, dated November 2013). The report, as previously outlined, states that the site has been previously developed and that there will have been some impact on any archaeological remains that may be present. However it concludes that the proposed development is of such a scale that it raises the prospect of unexpected remains of any period being present.

88. The site lies on the fringes of the Loddon valley, close to its confluence with the River Thames. Both valleys were a focus of prehistoric settlement and agriculture. Locally this is evidenced by the number and frequency of prehistoric finds spots, even though much of suburban Woodley was constructed with little or no archaeological investigations or observations.

89. Berkshire Archaeology therefore recommends that a condition requiring an archaeological investigation is attached to any planning permission granted, to mitigate the impact of the development.

### **Biodiversity:**

90. The Borough Ecologist has considered the impact of the development on protected species, as discussed in the submitted Ecological Survey report (MWA, Ref: 3486, 16 December 2013. Paragraphs 6.4.11 and 6.4.12 of the report give appropriate avoidance measures to avoid conflict with breeding birds. These can be included as a condition in order to secure protection for this species group. An informative will also be placed on the permission in the event that reptiles are found during the works.

### **Infrastructure Mitigation & Affordable Housing:**

#### Service, Amenity and Infrastructure impact:

91. In line with policies CP4 of the Core Strategy and the Planning Advice Note, contributions would be required for this scheme based on the provision on a net provision of 68 new dwellings. The applicant is currently finalising a S106 agreement with the Council for this scheme. The required level of contribution for this scheme is:

Affordable housing	29.4%
Affordable Housing comm.	
Sum	£43,000.00
Education	£719,161.00
Leisure, recreational and sports facilities	£191,090.92
Country parks, access and biodiversity	£39,304.00
Libraries	£15,590.36
Highways	£266,500.00
Air quality monitoring and assessment	£0.00
Thames Basin Heathlands SPA	£0.00
Monitoring fee	£13,600.00
Legal costs (minimum)	£1,000.00
<b>TOTAL:</b>	<b>£1,289,246.28</b>

92. Thames Valley Police have requested contributions to cover impact on police infrastructure. However in light of appeal decision APP/X0360/A/11/2151409, the request for funding by the TVP fails the tests set out in CIL Regulation 122. The lack of a contribution therefore does not justify the refusal of planning permission.

#### Affordable housing

93. The threshold for affordable housing is 5 dwellings or more or residential sites of 0.16 hectares or larger. The site exceeds this threshold and therefore there is a requirement for the provision of affordable housing.

94. To meet the requirements of Policy CP5 of the Core Strategy, a minimum of 30% of the total number of units (net) need to be provided as affordable housing. This equates to 20.4 units out of the proposed 68 dwellings. The developer is proposing 20 affordable housing units on site and the remaining 0.4 units as a commuted sum. Based on the Viability Study undertaken by Levvel Ltd, the Council's approach to calculating

commuted sums for affordable housing is based on the difference in the residual development value of a scheme without on-site affordable housing and the same scheme with on-site affordable housing. The commuted sum sought, in-lieu of 0.4 units, is £43,000 index-linked towards affordable housing in the borough.

95. The Housing Strategy Officer is agreeable to the following proposed mix of 20 units on site:

3 x 1 bed flats	Shared Ownership
6 x 2 bed flats	Shared Ownership
2 x 2 bed houses	Shared Ownership
5 x 2 bed houses	Social Rent
2 x 3 bed houses	Social Rent
2 x 4 bed houses	Social Rent

96. There is a focus on flats for shared ownership and the houses for social rent as this would help meet the priority need for family sized rented properties in Woodley. As such no objection is raised.

## CONCLUSION

The application is a full application and proposes 68 dwellings with associated roads, parking, amenity space, landscaping. The site was included in policy SAL02 of the Managing Development Delivery Local Plan (MDD) as an allocated housing site.

The application has attracted a number of local objections, principally in respect of the proposed new access. However the Highway Authority considers the safety of the access and traffic implications to be acceptable.

The site adjoins commercial uses to the north of the site, and the proposed mitigation measures ensure an acceptable noise environment for the proposed residential development, whilst also building in safeguards for the future should activity change in nature or increase in activity.

The proposed number of dwellings represents an appropriate density of development (32.4 dph) providing space for adequately sized gardens and adequate landscaping within the site.

It is therefore considered that the scheme is compliant with the development plan and as such is recommended for conditional approval.

## CONTACT DETAILS

Service	Telephone	Email
Development Management and Regulatory Services	0118 974 6428 / 6429	development.control@wokingham.gov.uk



Allied Bakery Site,

Viscount Way, Woodley.

SITE LOCATION PLAN.

SO-003-SLP-01. A.

1:1250 @ A3.

THIS DRAWING IS TO BE READ IN CONNECTION WITH SEPARATE GROUP / SITE SPECIFIC CONSTRUCTION NOTES AND MATERIALS SPECIFICATION.

Type	Accommodation	Size (sq ft)	No.
1333 SP	1 bed house	941	18
209	2 bed house	900	3
309	3 bed house	1190	4
501	5 bed house	1541	2
606 SP	6 bed house	1783	4
610	6 bed house	1783	4
615	6 bed house	1783	4
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995	6 bed house	1783	4
TOTAL			68

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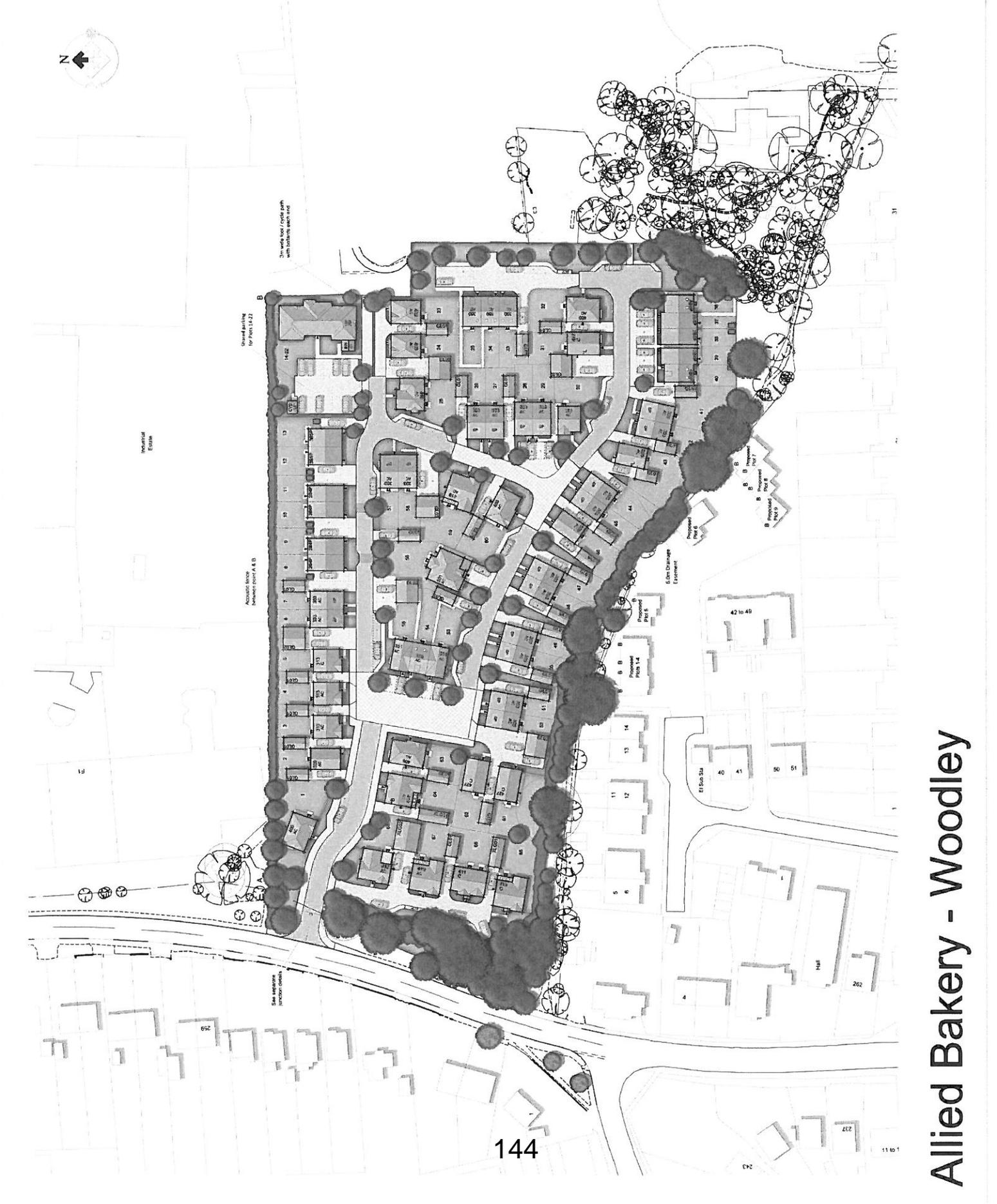
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1 of 11

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E-Sub-Station

Proposed Plant 1-4

Proposed Plant 5-10

Proposed Plant 11-15

E-Drainage Element

Acoustic fence between Court A & B

2m wide foot/cycle path with bollards each side

Industrial Estate

North Arrow

Scale: 1:500

Date: Dec'13

Drawn: PAS

Client: Allied Bakery Woodley

Project: Allied Bakery Woodley

Drawn: PAS

Client: Allied Bakery Woodley

Project: Allied Bakery Woodley

Drawn: PAS

Client: Allied Bakery Woodley

Project: Allied Bakery Woodley

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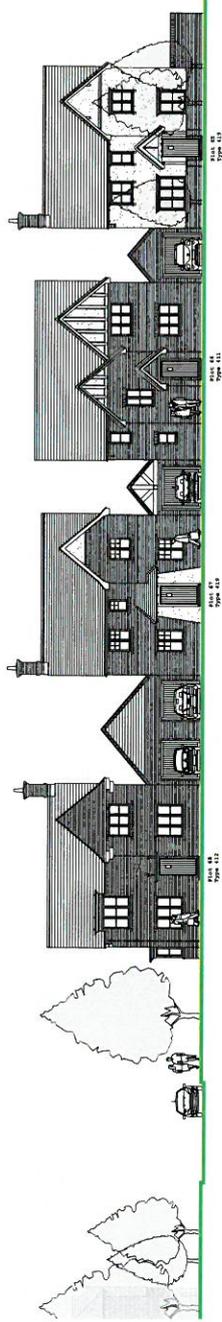
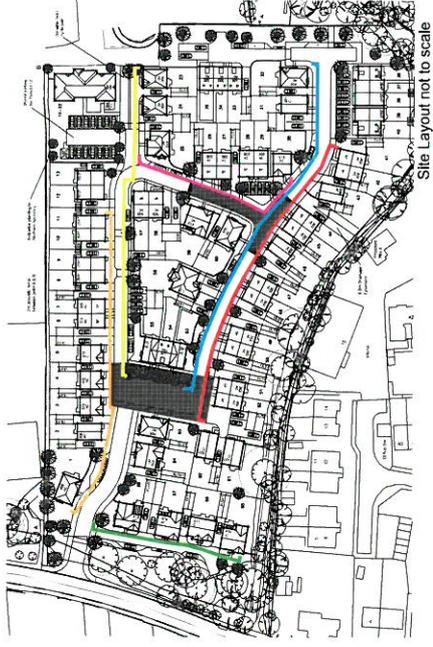
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Client: Allied Bakery Woodley

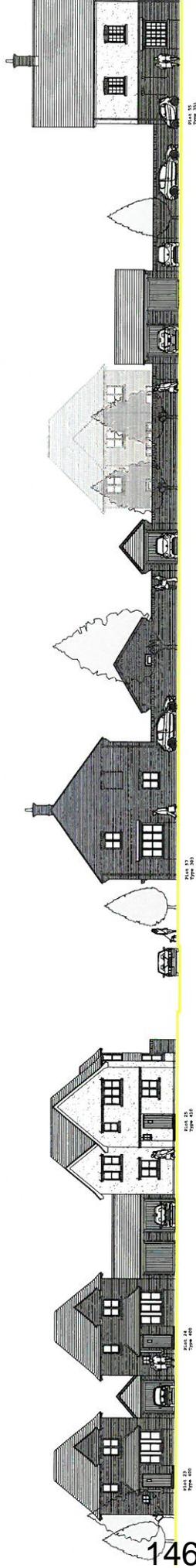
Project: Allied Bakery Woodley



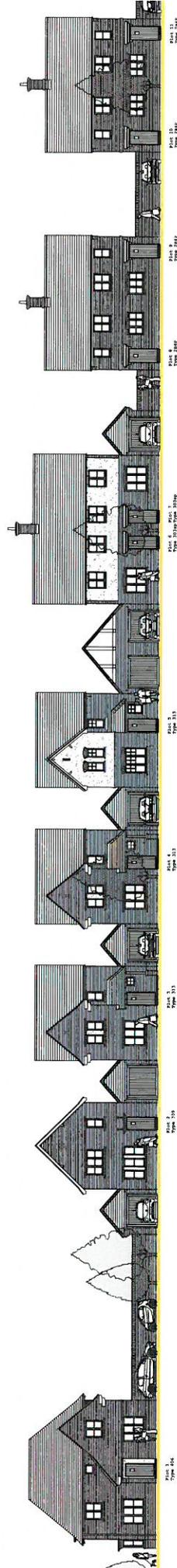
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Indicative street scene for Plots 65-68



Indicative street scene for Plots 23-25, 55, 57



Indicative street scene for Plots 1-11



Drawing Title:

STREETSCENES

Date: JAN'15

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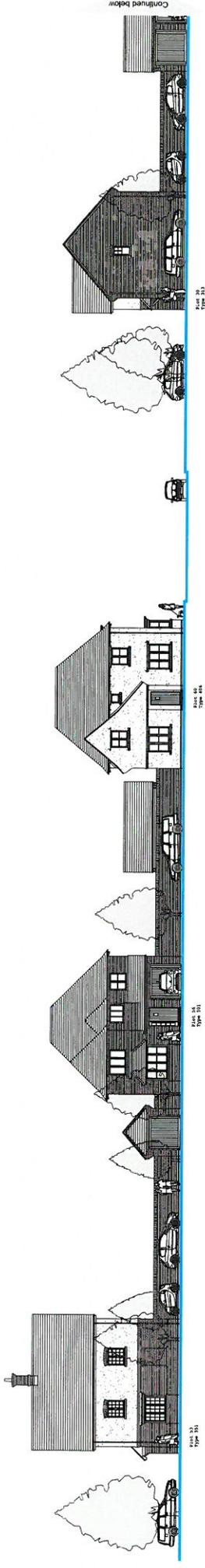
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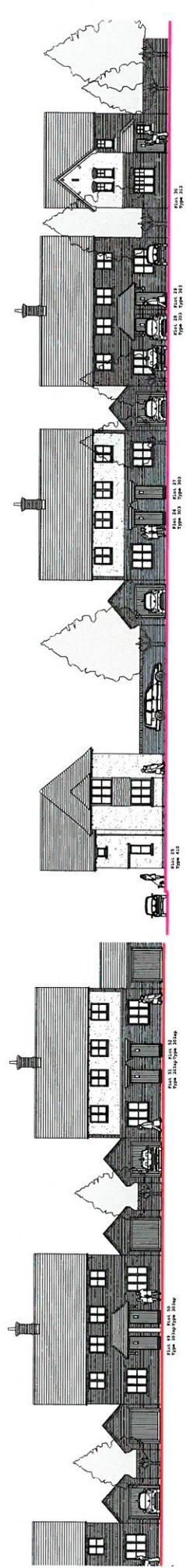
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Indicative street scene for Plots 30-32, 53, 56, 60



Indicative street scene for Plots 41-52



Indicative street scene for Plots 25-30



Drawing Title:  
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**Allied Bakery, Woodley**

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## Woodley Town Council

Clare Lawrence - Team Leader Development Management  
Wokingham Borough Council  
Shute End  
Wokingham  
Berkshire

Deborah Mander - Town Clerk  
The Oakwood Centre  
Headley Road  
Woodley  
Berkshire  
RG5 4JZ

Telephone 0118 9690356

Fax

Case Officer : David Wetherill

Date 24/10/2014

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**Application No F /2014/2105**

Type : Full

Status : 0 New Application

**Date Received : 10/10/2014**

Applicant : Bloor Homes Ltd & ABF Grain Pd  
River View House  
First Avenue  
Newbury Business Park  
Newbury, Berkshire  
RG14 2PS

Agent/Architect :

Location : Former Allied Bakeries Site  
Viscount Way  
Woodley  
RG5 4BJ

Parish :

N.G.R. :

Road Class :

Proposal : Proposed erection of 70 dwellings with associated roads, parking, amenity space, landscaping and creation of new access onto Loddon Bridge Road.

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### TOWN AND COUNTRY PLANNING ACT 1990 - LOCAL COUNCIL OBSERVATIONS

Woodley Town Council have considered the Application No F /2014/2105 and observations thereon are as follows :

The Committee considered the proposals and recommended the application be refused on the following grounds:

- There are safety concerns regarding the siting of the access on Loddon Bridge Road. Residents of Loddon Bridge Road in the vicinity of the proposed entrance to the site already find it very difficult and dangerous to exit their driveways due to the amount of traffic and poor visibility caused by a bend in the road. This will also apply to people exiting the proposed development. The Committee did not think it appropriate for there to be any access onto Loddon Bridge Road and felt strongly that the access to the development should be from Viscount Way.
- Many school children use this area of Loddon Bridge Road, on bicycles and on foot, and their safety will be put at risk by traffic using the proposed site entrance.
- As the proposed entrance is next to a blind bend in Loddon Bridge Road, traffic exiting the site will find it difficult to turn right, particularly during rush hours and most will therefore turn left. Much of this traffic will then turn into Vauxhall Drive to avoid the bottleneck at the southern end of Loddon Bridge Road, thereby exacerbating the traffic problems in Vauxhall Drive.
- The Wokingham Borough Managing Development delivery Document (Local Plan), adopted 21 February 2014, identified this site as appropriate for the delivery of around 57 dwellings. This proposal is for 70 dwellings.
- There is insufficient parking provided on the site.
- There appears to be footpath access to the rear of some properties, which does not appear to be in compliance with Safer Homes Initiatives.
- The acoustic fence to the north of the site does not appear to be appropriate for the nature of the business carried out in the industrial estate. The proposed acoustic fence is 2m high, whereas the HGV vehicles using the industrial estate are 4m high.
- The proposed 2m fence to the north of the site will not provide sufficient protection to the industrial units, where high value goods are sometimes stored.
- The noise impact assessment gives the measured noise levels from the 24 hours a day truck movements on the industrial estate, and general traffic noise, and states that in order for acceptable internal levels of noise to be achieved the windows of the dwellings adjacent to the industrial estate must remain closed day and night. This is not acceptable.

If permission is granted for this development the Committee request that the following concerns be addressed:

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## Woodley Town Council

Clare Lawrence - Team Leader Development Management

Wokingham Borough Council  
Shute End  
Wokingham  
Berkshire

Deborah Mander - Town Clerk

The Oakwood Centre  
Headley Road  
Woodley  
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RG5 4JZ

Telephone 0118 9690356

Fax

Case Officer : David Wetherill

Date 24/10/2014

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- High level windows in the industrial units to the north of the site will overlook the adjacent houses and gardens.
- Cctv cameras covering the industrial estate will overlook the adjacent houses and gardens.
- The HGV vehicles using the industrial estate have cabs at a height of 3m, which will overlook the adjacent houses and gardens.
- The existing business operating to the north of the site has to renew its 24 hour HGV operator's licence every 5 years and should be protected against objections by the new residents in order to comply with the Wokingham Borough Managing Development Delivery Document (Local Plan), which for this site specifically states that "the proposals must deliver appropriate measures to safeguard the amenities of the occupants of the dwellings whilst ensuring continued B class activities can continue within adjoining Headley Road Core Employment Area."
- The boundary treatment to the south of the site must provide protection to the historic bridle way in this area.

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<b>Development Management Ref No</b>	<b>No weeks on day of committee</b>	<b>Parish</b>	<b>Ward</b>	<b>Listed by:</b>
F/2014/2865	10	Wokingham Without	Wokingham Without	Cllr Pauline Helliar-Symons

**Applicant** Mr Jason Brand C/O The Edwards Irish Partnership

**Location** Land at Heathlands, Heathlands Road, Wokingham **Postcode** RG40 3AS

**Proposal** Proposed erection of two no. bedroom single storey dwelling, to include the construction of a new roof on existing garage, following demolition and removal of existing garage block

**Type** Minor - Dwellings

**PS Category** 13

**Officer** Nick Chancellor

**FOR CONSIDERATION BY** Planning Committee on 04/03/2015

**REPORT PREPARED BY** Head of Development Management and Regulatory Services

## SUMMARY

The application site is in designated Countryside and fronts Heathlands Road, just to the south of Bevere Lodge and an access road serving Heathlands Court. The site is currently relatively open and hosts two garage blocks, the larger of which is set back from the highway by 11m.

The proposal seeks planning permission for the erection of a two bedroom lodge style bungalow property, following the demolition of an existing garage block. A smaller, existing garage block on the site would be converted to form a garage outbuilding to serve the new dwelling.

The application has been assessed against the relevant policies of Wokingham Borough Development Plan and account has been taken of other relevant material considerations. The report concludes that the proposal would not accord with Core Strategy Policy CP6 (Managing Travel Demand) and CP11 (Proposals outside Development Limits (including countryside)) as the site is isolated, leads to an undesirable increase in scale and change in character and does not constitute environmental improvement. The Tree and Landscape Officer has advised that the development would require replacement planting of TPO trees previously removed from the site, which would be at odds with future occupiers' expectation for an open, sunny garden.

Two previous and similar applications on the site have been dismissed at appeal and are notable material considerations. A planning application for a similar application pertaining to the same site was refused in November 2014. The current application is not considered to be a significant improvement such that the unsustainable location and harm to the countryside could be justified. As such it is recommended that planning permission is refused.

In reaching this conclusion due consideration has been given to whether the use of

landscaping conditions would be possible to mitigate the impact of the proposal or would out-weigh the above concerns. It is concluded that in this instance this would not be the case.

The development would require the creation of a safe access, but this could be ensured through planning conditions and so should not form further reasons for refusal. The development would also be required to make arrangements for the improvement or provision of infrastructure, services, community and other facilities. While a section 106 legal agreement has not been pursued due to the recommendation for refusal, this would be required and is included as an additional reason for refusal.

#### **PLANNING STATUS**

- Outside of Development Limits (Countryside)
- Potentially Contaminated Land Consultation Zone
- Special Protection Area (5km zone)
- Area Tree Protection Order (442/1989)
- SSSI 500m Consultation Zone
- Farnborough Aerodrome Consultation Zone
- Wind turbine consultation zone

#### **RECOMMENDATION**

REFUSE planning permission for the following reasons:

1. Due to the increase in scale and the site's isolated location, the proposed dwelling is a form of unsustainable development that would urbanise the application site; to the detriment of visual amenity, the character of the area, the intrinsic character and beauty of the countryside and the separate identity of neighbouring settlements. The application is therefore contrary to the requirements of the National Planning Policy Framework and Core Strategy Policies CP1, CP3, CP6 and CP11.

2. In the absence of a planning obligation, the proposal does not secure appropriate mitigation of the service, amenity and infrastructure needs which would result from the development. As such, the proposal increases pressure on existing services and infrastructure and is therefore contrary to Policies CP1, CP3 and CP4 of the adopted Wokingham Borough Core Strategy.

3. In the absence of SPA contributions, the Planning Authority is unable to satisfy itself that the proposal would not have an adverse impact on the integrity of the Thames Basin Heaths SPA. Accordingly, since the planning authority is not satisfied that regulation 49 of the Conservation (National Habitats etc) Regulations 1994 applies in this case, it must refuse permission in accordance with regulation 48(5) of the 1994 Regulations and Article 6(3) of Directive 92/43/EEC. Furthermore, the proposal conflicts with Policy CP8 of the Core Strategy.

#### **PLANNING HISTORY**

32663 – Erection of a 2 bedroom lodge following demolition of garage. Refused 1989, appeal dismissed 19/02/1990.

98/68309/F – Proposed erection of detached dwelling with double garage and

demolition of existing garages. Refused 28/10/1998.

F/2000/1935 – Demolition of existing garage and erection of (two storey) dwelling with garage. Refused 18/10/2000.

F/2001/3221 – Demolition of existing garages and erection of a (bungalow) dwelling with detached garage. Refused at planning committee 03/05/2001, appeal dismissed 09/08/2001.

Officer note: The F/2001/3221 appeal decision is a material consideration for the assessment of the current application. Due to the very similar nature of the development proposed (also for erection of a lodge/bungalow dwelling), it should be attributed significant weight in the balancing process.

The Inspector's report concluded that *"the tailored, open conditions associated with a residential use would make the dwelling far more conspicuous ... I consider that it would look incongruous in this rural setting and damaging to the countryside amenities that the development plan seeks to protect. In my judgement the development would not enhance the rural environment"*.

The appeal scheme was clearly found to be unacceptable by the Inspector. Therefore, the current application will be assessed in terms of whether it offers material improvement, taking into account current Development Plan policies.

F/2004/2033 - Proposed erection of security fencing across site access. (Retrospective). Refused 12/07/2004.

F/2014/2081 - Proposed erection of a single storey detached dwelling, addition of pitched roof over retained single detached garage and blocking off of existing access and formation of a new access. Demolition of existing garage block. Refused 04/11/2014.

Officer note: Very similar application, but with 0.75m taller roof ridge (6.1m instead of 5.35m). The current application is therefore assessed in terms of whether it has overcome the reasons for refusal, cited below:

1. Due to the increase in scale, the proposed dwelling is a form of unsustainable development that would urbanise the application site; to the detriment of visual amenity, the character of the area, the intrinsic character and beauty of the countryside and the separate identity of neighbouring settlements. The application is therefore contrary to the requirements of the National Planning Policy Framework and Core Strategy Policy CP11.

2. In the absence of a planning obligation, the proposal does not secure appropriate mitigation of the service, amenity and infrastructure needs which would result from the development. As such, the proposal increases pressure on existing services and infrastructure and is therefore contrary to Policies CP1, CP3 and CP4 of the adopted Wokingham Borough Core Strategy.

3. In the absence of SPA contributions, the Planning Authority is unable to satisfy itself that the proposal would not have an adverse impact on the integrity of the Thames Basin Heaths SPA. Accordingly, since the planning authority is not satisfied that regulation 49 of the Conservation (National Habitats etc) Regulations 1994 applies in this case, it must refuse permission in accordance with regulation 48(5) of the 1994 Regulations and Article 6(3) of Directive 92/43/EEC. Furthermore, the proposal conflicts with Policy CP8 of the Core Strategy.

4. The proposals fails to demonstrate that the development would seek to achieve the requirements of the full Code for Sustainable homes Level 4 and would therefore be contrary to MDD Policy CC04.

**SUMMARY INFORMATION**

Site Area	0.07 Hectares
Existing units	Two garage structures
Proposed units	One dwelling
Number of bedrooms	Two
Number of affordable units proposed	Zero
Existing land use	Lock up garages ( not ancillary to primary use): Sui Generis
Proposed land use	C3 Dwellinghouse
Public open space proposed	None
Proposed parking spaces	2 – parking/turning area shown on plan

**CONSULTATION RESPONSES**

WBC Highways	<p>No technical objection subject to conditions to ensure parking and turning provision, cycle parking, no gates at site entrance, appropriate surfacing and access visibility. Only acceptable subject to an appropriate highways s.106 contribution.</p> <p>However, the highways officer notes that the development is not in a sustainable location, due to it not being within an existing settlement or within close proximity to local services to meet the everyday needs of future occupiers.</p>
WBC Ecology	No objection subject to informative re: native reptiles
WBC Trees and Landscape	<p>Objection due to incompatibility between the proposed residential use and area TPO corresponding to the site.</p> <p>Given the extent of replacement TPO planting that would be required (as per s.206 of the Town and Country Planning Act 1990) and the reasonable expectation from future residents for an open and sunny garden space</p>
Environmental Health	No comment

**REPRESENTATIONS**

Ward members	Cllr Pauline Helliard-Symons
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	<p><i>"In the past this application has failed under pre NPPF regulations as an additional dwelling in the countryside despite the benefits of removing the unsightly garages. The proposed bungalow would be an improvement to the amenity of the whole area, and the applicant's architects have taken a constructive approach to the design. The application last year was refused on 4 counts, the last three being administrative - i.e S106, SPA, Code 4. The substantive objection does not in our view hold water - 'Due to increase in scale, the proposed building is a form of unsustainable development that would urbanise the application site.....'. Quite the reverse is the case since the scale would overall appear to be less. It meets the current NPPF criteria such as 'Core Planning Principles' to 'seek to secure high quality design', 'take account of different roles and character of different areas...', 'not simply be about scrutiny but instead be a creative exercise in finding ways to enhance and improve the places in which people live their lives'. Also in delivering sustainable development. It will without doubt improve the appearance of this corner, which is very visible. Thus whilst our Core Strategy was adopted before the NPPF came in, this application meets CP1, 3, and would provide a single dwelling suitable for its siting and an appropriate exception site for these reasons.</i></p> <p><i>The proposal therefore seems to be in keeping with the area, does not detrimentally affect adjoining Beveridge Lodge, and in particular has the support of the local residents; and there is now a prejudice in favour of local people's views having more influence. It enhances the area by ensuring an appropriate use which will maintain the site in an attractive condition. Overall there will be a net gain to the amenity of the area, and these environmental factors outweigh any possible harm that planning officers might understandably be concerned about."</i></p> <p>Cllr David Sleight:</p> <p><i>"Despite the planning history of applications for building on this site, I consider that this application should be approved. It is a small well designed cottage that would complement the rural scene in Heathlands Road and has, I understand, the support of the residents of Heathlands Court, the gated development to the rear of the application site.</i></p> <p><i>I know its location on Heathlands Road and consider that any traffic entering or exiting the site will be able to do safely although, should you recommend the application for approval, you may wish to consider some restriction on builders' vehicles waiting on Heathlands Road."</i></p>
Wokingham Without Parish Council	"On this occasion Wokingham Without Parish Council recommend APPROVAL of this application.

	<p>This is as a REPLACEMENT dwelling of the garage block that is already in situ on a Brownfield site. As a rule Wokingham Without Parish Council DO NOT support infills within the parish.”</p> <p>Officer note: The application is <u>not</u> for a replacement dwelling as there is no existing dwelling on the site.</p>
Local residents	<p>16 letters received – all in support of the application.</p> <p>The majority of these responses note the present ugly/untidy state of the site and consider that redevelopment would greatly improve its appearance.</p> <p>Responses also include (in summary):</p> <ul style="list-style-type: none"> <li>- Previous nuisances such as use of the site for a storage container, fly-tipping, temporary stationing of a caravan, parking of vehicles, fallen trees.</li> <li>- A dwelling on the site would mean that it is maintained</li> <li>- Need for bungalow properties in the Borough</li> <li>- Opportunity to re-use a brownfield site</li> <li>- Landscaping proposed seems sympathetic</li> <li>- There are other dwellings nearby</li> <li>- Pragmatic solution</li> <li>- Character of Heathlands Road has altered in recent years, a new bungalow would be consistent with the direction of travel.</li> <li>- NPPF states planning process should be a “creative exercise in finding ways to enhance and improve places people live their lives”.</li> <li>- The existing garages intrude on an area which is clearly residential</li> <li>- Design of the dwelling is modest and would be a pleasant addition</li> </ul>

### **APPLICANTS POINTS**

The applicants Design and Access Statement concludes that the proposed dwelling is sympathetically designed in terms of layout, built form, height and will make a positive contribution to the long term character of the area without eroding the Countryside. Existing and proposed tree planting would screen and soften the road frontage, integrating the dwelling with its surroundings.

### **PLANNING POLICY**

#### NATIONAL

National Planning Policy Framework

#### LOCAL

Development Plan: -

Core Strategy 2010

- CP1 – Sustainable development
- CP3 - General Principles for development
- CP4 - Infrastructure Requirements
- CP6 - Managing Travel Demand
- CP7 – Biodiversity
- CP9 – Scale and location of development proposals
- CP11 – Proposals outside development limits (including countryside)

Managing Development Delivery Local Plan (Adopted Feb 2014)

*Cross-Cutting Policies*

- CC01 – Presumption in Favour of Sustainable Development
- CC02 – Development Limits
- CC03 – Green Infrastructure, Trees and Landscaping
- CC04 – Sustainable Design and Construction
- CC07 - Parking

*Topic-Based Policies*

- TB07 – Internal Space Standards

Guidance: -

- Borough Design Guide SPD 2012
- Sustainable Development and Construction SPD 2010
- Landscape Character Assessment SPG 2004
- Planning Advice Note: Infrastructure Impact Mitigation Contributions for New Development (Revised November 2010)

**PLANNING ISSUES**

**Principle of Development:**

1. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. Managing Development Delivery Local Plan Policy CC01 states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.
2. There are three dimensions to sustainable development: economic, social and environmental. The NPPF stipulates that the environmental role is intended to protect and enhance our natural environment. However, the environmental role should not be undertaken in isolation, because they are mutually dependent. To achieve sustainable development, economic, social and environmental gains should be sought jointly and simultaneously through the planning system.
3. NPPF paragraph 17 states that planning should encourage the effective use of land by reusing previously development (brownfield land), provided it is not of high environmental value. Development should take into account the roles and character of different areas, recognising the intrinsic character of the countryside.

4. Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. The NPPF does not change the statutory status of the development plan as the starting point for decision making. Proposed development that accords with an up-to-date Local Plan should be approved and proposed development that conflicts should be refused, unless other material considerations indicate otherwise. The WBC Core Strategy and Managing Development Delivery LP constitute an up to date Development Plan, accord with the NPPF, and are therefore the starting point for decision making.
5. Development Limits are defined within the policies map associated with MDDL Policy CC02. The application site is located within the countryside where development plan policies seek to ensure the protection, and where possible the enhancement of the open and rural character of such areas.
6. New dwellings within the countryside would not normally be permitted and are only be acceptable in principle subject to an assessment of the potential impacts on the open and rural character of the countryside. Core Strategy Policy CP11 supports the principle of replacement buildings outside development limits only in cases where environmental improvement can be demonstrated. Development should not result in inappropriate increases in the scale, form or footprint of the original building
7. Policy CP3 states that development must be appropriate in terms of its scale of activity, mass, layout, built form, height, materials and character to the area in which it is located and must be of high quality design without detriment to the amenities of adjoining land uses and occupiers. Policy CP6 states that development should be located where they are (or will be at the time of development) choices in the mode of transport available and which minimise the distance people need to travel.
8. For reasons outlined in the body of this report, the application proposals constitute inappropriate development within the countryside and are therefore in conflict with Development Plan policies. There are no other material considerations weighing in favour of the proposal. Therefore, planning permission should be refused.

#### **Planning History:**

9. Planning application 32663 (originally submitted 1989) sought the erection of a single storey detached dwelling on the application site. The application was refused by the Local Authority and later dismissed at appeal. In doing so, the planning inspector noted that, although the dwelling would have been partially screened by vegetation that was on the site at that time, it would *“be visible through the trees going south, and because it would be near to the road and to the lodge would have the character of ribbon type development, which would be intrusive and harmful to the surrounding area”*.
10. A similar planning application for a lodge style bungalow dwelling was submitted in 2001 under application reference F/2001/3221. The proposed dwelling was similarly sized to the current application, although positioned at right angles to the road and facing north. The application was refused and subsequently dismissed at appeal.
11. In commenting on the F/2001/3221 appeal scheme, the Inspector identifies that “the

development proposed is the erection of a single storey lodge house with single garage". Consideration was given to the (then) development plan policies, including Policy C2, which indicated that development may be permitted *"for the redevelopment of existing buildings ... to enhance the rural environment ... provided it is appropriate ... in the countryside"*.

12. The F/2001/3221 appeal scheme was clearly found to be unacceptable. The Inspector's report concluded that *"the tailored, open conditions associated with a residential use would make the dwelling far more conspicuous ... I consider that it would look incongruous in this rural setting and damaging to the countryside amenities that the development plan seeks to protect. In my judgement the development would not enhance the rural environment"*.
13. Although C2 no longer forms part of the Development Plan, current Core Strategy Policy CP11 is very similar. Development within countryside will not normally be permitted, unless certain criteria are met (see 'Principle of Development' above).
14. A more recent application, again for a lodge style bungalow dwelling, was submitted in under application reference F/2014/2081. The application was refused in November 2014 by the Head of Development Management under delegated authority, as the development was considered to urbanise the site, to the detriment of visual amenity, the character of the area, intrinsic character of the countryside and separate identity of neighbouring settlements. The Borough's adopted planning policies (as regards countryside) were considered sufficiently similar to those used to assess previous applications. The application was in conflict with adopted policy as regards the scale and location of development and was not considered to be an exception to Core Strategy Policy CP11 in terms of environmental improvement. The current application is highly similar (the only significant difference being that the roof ridge height is now shown 0.75m lower at 5.35m instead of 6.1m).

#### *Summary*

15. The planning policy context has not changed significantly since the two previous appeals were dismissed at appeal and a very similar recent planning application was refused under current development plan policies. The planning history is considered to weigh heavily against the current application. Were the current application to be approved, the decision would need to be justified against this history.

#### **Impact on the character of the area:**

16. As with the two appeal schemes outlined above, the current application is for a bungalow / lodge dwelling and for the demolition of the main (larger) garage block. However, it would be sited in a different location within the plot.
17. As with the recent F/2014/2081 scheme, rather than facing the neighbouring Beveridge Lodge at an angle the dwelling is centrally located within the plot, approximately 2m forward of the existing main garage block and fronting Heathlands Road head on. However, the roof ridge as currently proposed would be 0.75m lower at 5.35m, whereas it was previously shown at 6.1m. The dwelling would be 14m wide by 6.5m deep and with a tiled, pitched roof. A front porch and gable feature would extend slightly forward of the main front building line. The elevations indicate an attractive lodge style property.

*Assessment against Local Policy*

18. At the time of the F/2001/3221 appeal, the site was described by the Inspector as being “*overgrown*”. The same cannot be said of the site now, although there are significant clusters of trees that remain around the edges of the site, including specimens that are protected under TPO 442/1989. Just as before, the new dwelling would have a more conspicuous appearance than the existing main garage block. This is exacerbated by its being sited 2m further forward towards Heathlands Road than the existing garages. What little development there is along this stretch of the road tends to be set well back and concealed by thick foliage. By introducing a more prominent structure in close proximity to the main road, the proposal is at odds with the surrounding rural, sylvan character of the area. While it is accepted that the impact could be softened through landscaping, the residential use would formalise and urbanise the countryside setting which would in turn detract from the separate identity of neighbouring settlements.
19. The proposed development does not qualify under any of the exceptions to Policy CP11, including the stipulation that replacement buildings must bring about “*environmental improvements*”. Given the prominent countryside location of the site along Heathlands Road, the site is considered to have significant environmental value due to its role in maintaining the existing rural character. This is reflected in the area TPO designation pertaining to the application site. Commenting on the F/2001/3221 appeal scheme, the Inspector also expresses concerns that the residential use would result in “*tailored, open conditions*” along the road frontage. Whilst planning conditions could be imposed to secure frontage hedge planting, such an arrangement only be guaranteed for the short to medium term, save for trees that are protected by TPO.
20. The dwelling would be 5.35m to the roof ridge and the proposed height represents an inappropriate increase in scale and form relative to the existing garages. The reduction of 0.75m since the previous F/2014/2081 application (down from 6.1m), is not considered a significant improvement that would justify residential development unsustainable location or the harm caused to the countryside. The previous reason for refusal (which refers to an inappropriate increase in scale) therefore remains relevant to the current application. If the application were to be approved then this would need to be justified in relation to the site planning history, including the recent F/2014/2081 decision.

*Assessment against the NPPF*

21. Consideration has been given to NPPF paragraph 17, which encourages the reuse of brownfield land, provided this land is not of high environmental value. However, this must be balanced against other core principles within the NPPF. For example, also set out within paragraph 17 is a requirement for development to take account of the roles and character of different areas, including the intrinsic character of the countryside.
22. Although the NPPF (para 55) post-dates the Core Strategy, the local policy does not rule out residential development in the countryside per se and is not in conflict with the Framework. Core Strategy CP11 similarly sets out special circumstances where development may be acceptable within the countryside; hence there is a very significant overlap between national and local policies.

*Comparison to previous Wokingham Borough Council policies*

23. The 2001 appeal for a similar development on the site was dismissed by the Inspector as it was not found to be consistent with the council's then planning policies, including superseded policy WCC1 because the development was within a designated 'green gap'. Though the council does not currently have a 'green gap' policy, the successor policy (CP11) also seeks to ensure separation between settlements. In dismissing a recent appeal decision for two new dwellings (1 net additional) at Paddock Farm, Nine Mile Ride (F/2013/1950) the Inspector noted that *"although the name has changed, the aims of the settlement policy remain ... similar or the same"*. The Inspector goes on to say *"although one more house would cause little harm in itself [i.e. to settlement separation], this argument could be used for each new house so that the cumulative effect would seriously undermine the aims of consolidating development in accessible locations and reducing reliance on the private car for most journeys"*.

24. Therefore, for the purposes of assessing the current application, the council's adopted policies can be considered to be in accordance with those that were previously used to assess the 2001 appeal scheme. The dismissal of that appeal is therefore a significant material consideration for the current application.

#### *Area Tree Protection Order (TPO)*

25. It is noted that some (but not all) of the site's TPO trees have been removed since the "area" designation was first made. Had the principle of development otherwise been found to be acceptable, the Tree and Landscape officer has advised that replacement (like for like) planting of previously removed TPO trees should be sought by condition. However, the officer has also advised that an equivalent intensity of replacement planting would be incompatible with a residential use on the site, given its modest dimensions and reasonable expectation for open and sunny garden space from future residents.

#### *Summary*

26. Two previous appeal decisions for similar schemes have been dismissed and are notable material considerations for the current application. Current planning policies are equivalent to those previously used to appraise the site and the Inspector's previous analysis can be similarly applied.

27. The development would introduce a new dwelling in a prominent, unsustainable countryside location. While it is accepted that the existing main garage block may be unattractive, it does not follow that its substitution with a dwelling would bring about an environmental "improvement", since any such development should also be in keeping with the council's wider planning policies and aspirations, which seek to prevent urbanisation within the countryside and to protect the identity of neighbouring settlements. The brownfield status of the land and current unattractive appearance are not therefore overriding factors weighing in favour of the application proposal.

#### **Impact on Neighbours:**

28. The dwelling would be relatively well separated from its neighbours and would not result in any material loss of light, overlooking or overbearing impact.

#### **Amenity Space:**

29. Although no detailed calculations have been provided, the development appears to meet the council's internal space standards for a 2 bedroom/4 person dwelling.
30. Superficially, an adequate area of private garden amenity space is provided (in excess of the 11m garden length recommended within the Borough Design Guide). However, the extent to which the space would be 'usable' is questionable given the site layout and in view of the Tree Officer's stipulation that a significant number of replacement trees would be required in line with the area TPO.

#### **Highway Issues:**

31. The existing site access would be recreated further to the south. An acceptable quantum of parking has been provided. The Highways Officer raises no technical objection, subject to conditions.
32. However, the Highways Officer considers that site not to be in a sustainable location due to the relative distance to services and amenities, lack of a pedestrian route along Heathlands Road and reliance on the private car for journeys.

#### *Unsustainable location*

33. A nearby planning application for a new dwelling at Jalens, Nine Mile Ride was dismissed at appeal (application F/2013/0685, appeal reference APP/X0360/A/13/2203660). In dismissing the appeal, the Inspector noted that the application site was at the end of a long line of residential development, and though it was covered in hard standing and used for the parking of a number of vehicles, was also adjacent to undeveloped woodland. In dismissing the appeal the Inspector considered the site to be inappropriate due to there not being any local services within a comfortable walking distance of the site, concluding that the site "*would be isolated and therefore would not constitute sustainable development*".
34. The application site is within close proximity to the 'Jalens' appeal site. If anything, the application site constitutes an even more isolated location due to the absence of any pedestrian route along Heathlands Road. The application is for a new, rather than replacement dwelling and is therefore an unsustainable form of development.

#### **Ecology Issues:**

35. The Ecology Officer is satisfied that the ecology survey demonstrates that the development is unlikely to cause harm to the local reptile population. However, it is recommended that caution be exercised in the event that reptiles are found, so that appropriate mitigation can be put in place.

#### **Sustainability:**

36. MDD Policy CC04 states that planning permission will only be granted for proposals that seek to deliver high quality sustainably design and constructed developments. New homes should seek to achieve the requirements of the full Code of Sustainable Homes Level 4 and meet internal potable water consumption targets of 105 litres or less per person per day. Waste management and on site recycling should also be addressed in all development.
37. A code for sustainable homes pre-assessment report has been undertaken since the

refusal of the previous F/2014/2081 application and demonstrates that the development is capable of achieving the criteria for Level 4. The previous reason for refusal in relation to sustainability has therefore been overcome and is not included in the recommended decision. Had the development been otherwise acceptable, a condition would have been recommended to require the Level 4 to be achieved.

**Section 106 contributions**

38. In accordance with Core Strategy policy CP4 new development is expected to make arrangements for the improvement or provision of infrastructure, services, community and other facilities.

The Borough has adopted a Planning Advice Note on Infrastructure Impact Mitigation - Contributions for New Development (PAN)

The catchment Hatch Ride Primary school is currently over-subscribed and the development thus needs to contribute towards additional mainstream primary education in the area. The catchment Edgbarrow (Bracknell) school currently has surplus capacity and there is thus no need for the development to contribute towards the provision of additional mainstream secondary education in the area.

The highway authority has identified the need for local highway and public transport improvements to accommodate increased traffic in the area.

In accordance with the PAN the development would necessitate contributions as follows:

Education	£4,142.00
Leisure, recreation & sports facilities	£2,893.94
Country parks, biodiversity & countryside access	£578.00
Libraries	£229.27
Highways	£2,600.00
Thames Basin Heathlands SPA	£2,542.20
Monitoring fee	£200.00
Legal costs (minimum)	£1,000.00
<b>TOTAL:</b>	<b>£14,185.41</b>

**NOTES:**

No contributions sought for mainstream secondary school places; the catchment school currently having capacity.

39. Although the applicant has indicated a willingness to enter into a section 106 agreement, these contributions have not been pursued due to the scheme being considered unacceptable. On this basis, a further reason for refusal is necessary.

**CONCLUSION**

40. The development would introduce a new dwelling in a prominent, unsustainable countryside location. The application scheme does not constitute an exception to Core Strategy Policy CP11 as it would not bring about environmental improvement

that is consistent with the council's broader policy aspirations to prevent urbanisation of the countryside, protect the separate identity of neighbouring settlements and avoid the siting of new dwellings in isolated, car-dependant locations. The landscape officer has advised that necessary replacement planting of TPO trees is potentially inconsistent with future occupiers' reasonable expectation for an open, sunny garden. The development would urbanise the site, contrary to countryside policies and so a neater, tidier appearance does not constitute environmental improvement in this instance.

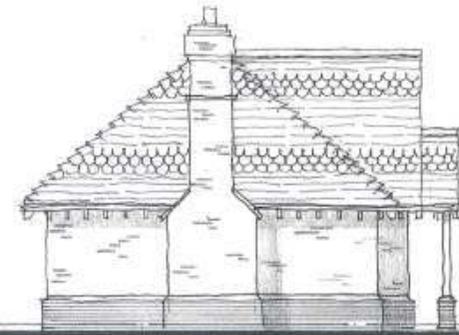
41. Two previous planning applications (32663 and F/2001/3221) were appraised under a similar countryside policy framework and were dismissed at appeal in each case. A very similar application was refused in 2014 under current development plan policies. It is not considered that the proposed 0.75m reduction in the roof ridge height would overcome the F/2014/2081 reason for refusal. If the application were to be approved then this would need to be justified in relation to the site planning history, including the recent F/2014/2081 decision.

42. As such it is recommended that planning permission is refused.

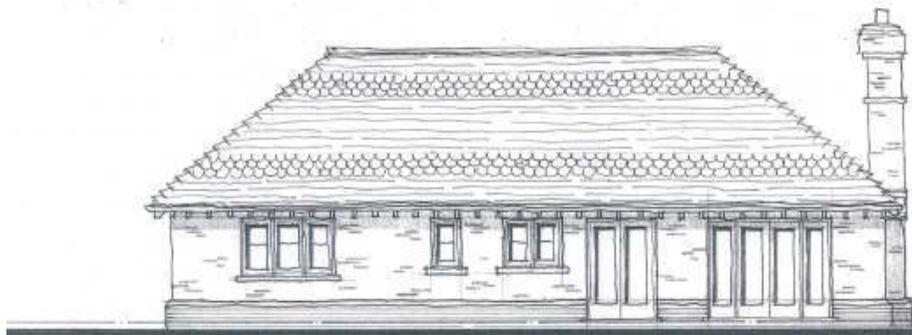
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Development Management and Regulatory Services	0118 974 6428 / 6429	development.control@wokingham.gov.uk



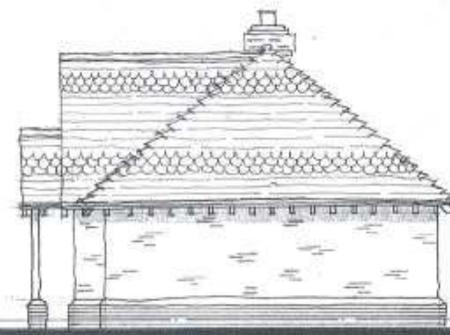
FRONT ELEVATION



SIDE ELEVATION

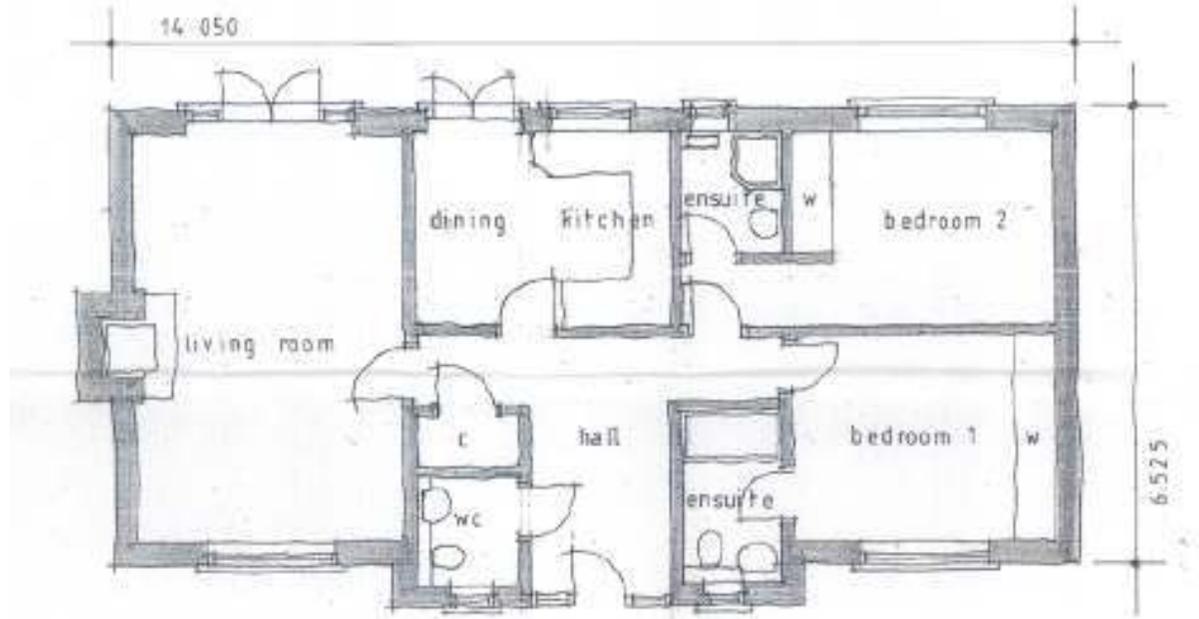


REAR ELEVATION



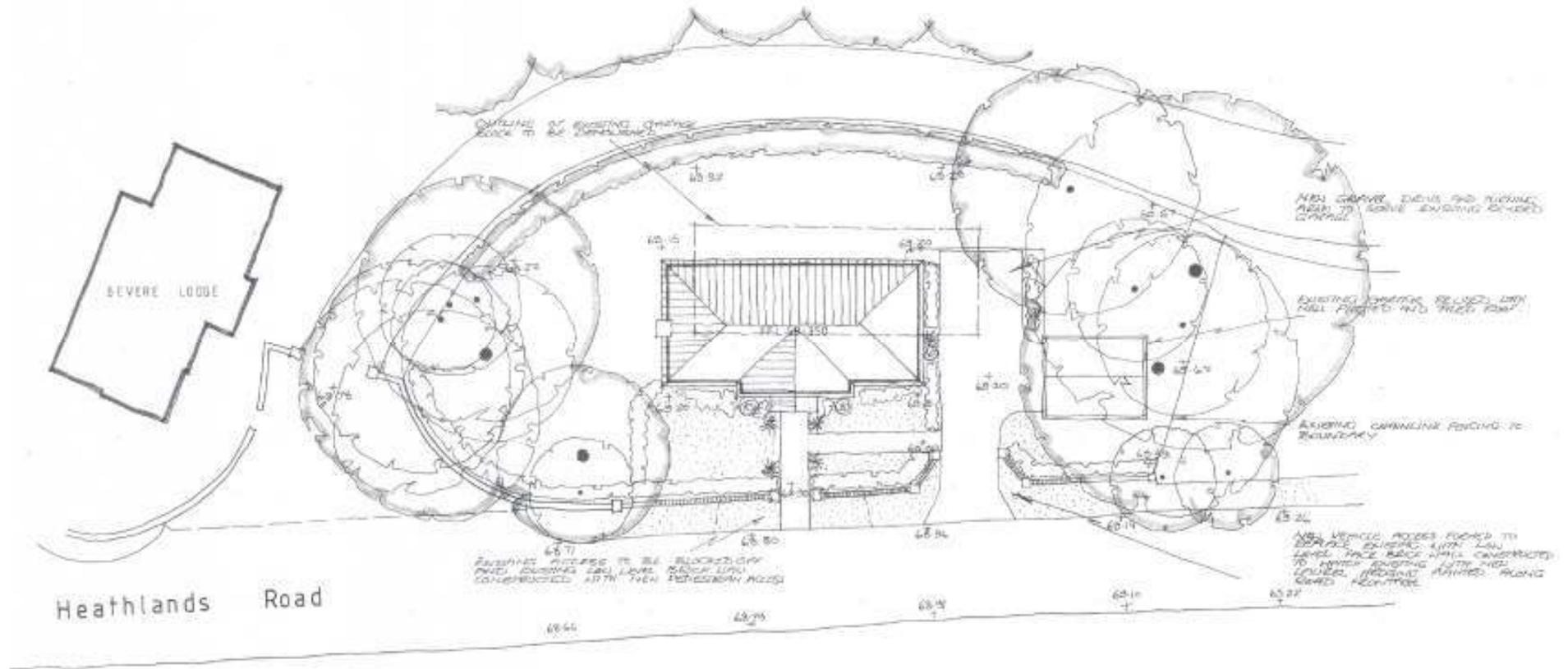
SIDE ELEVATION

Proposed elevations

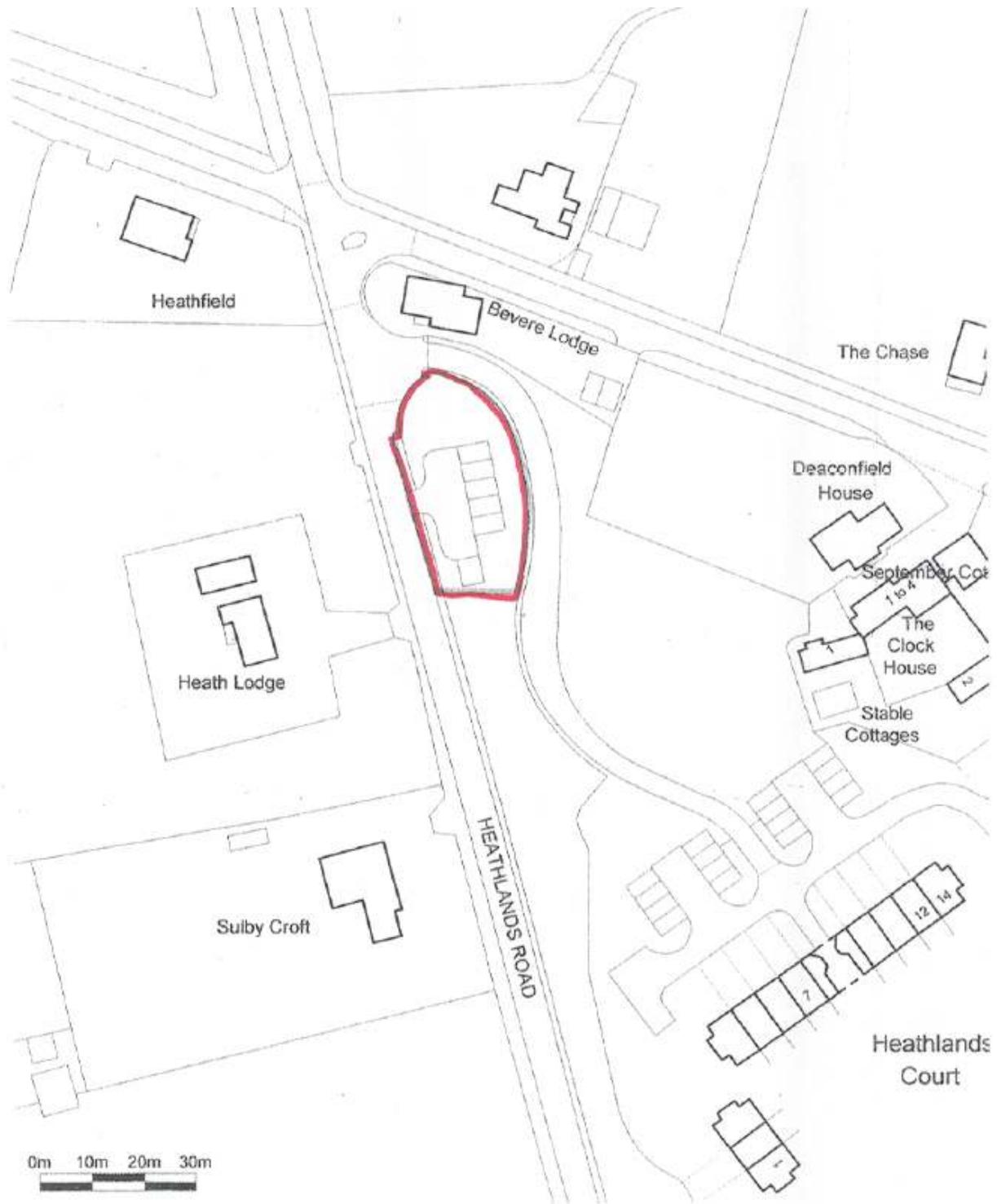


FLOOR PLAN

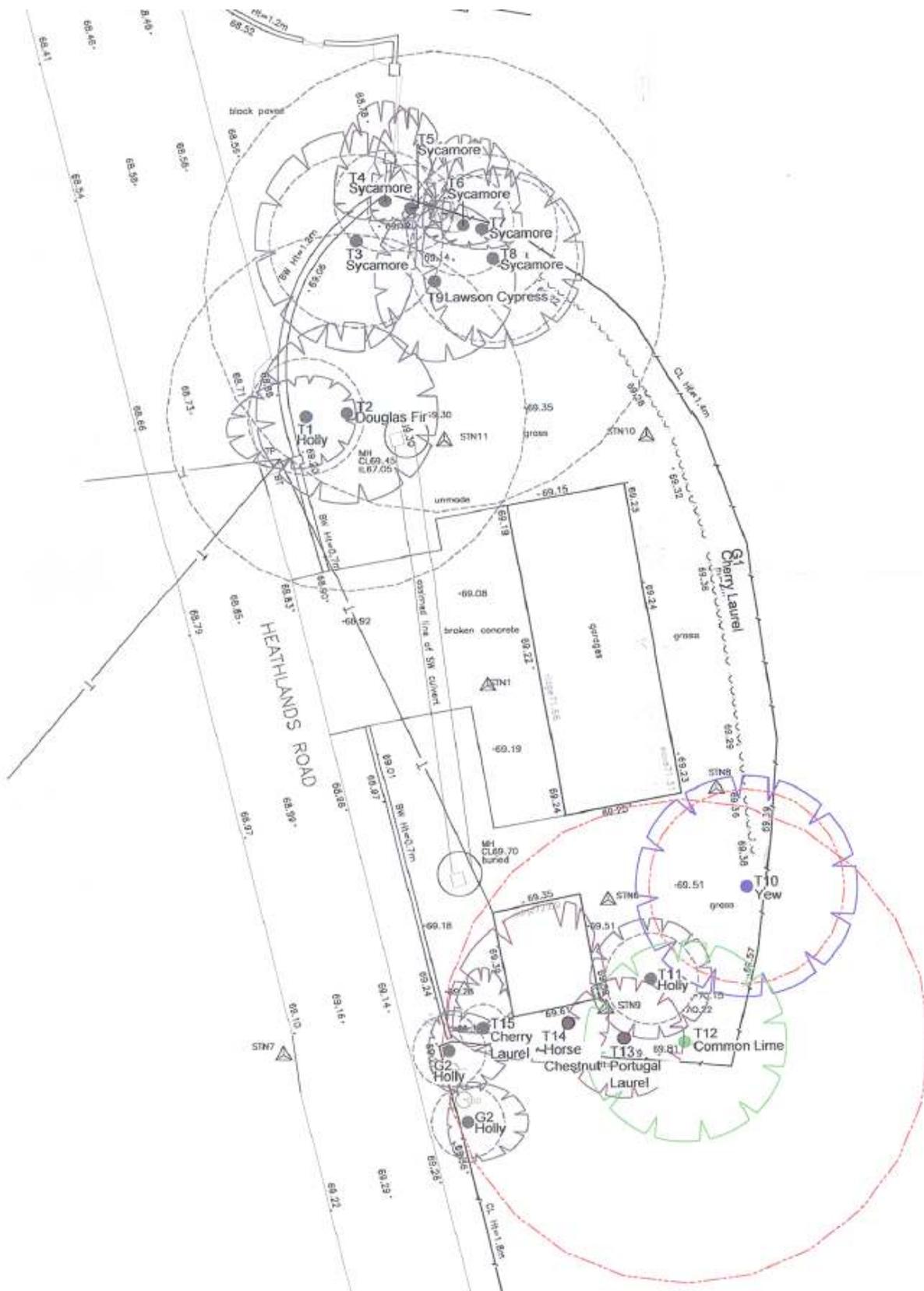
Proposed dwelling floor plan



Proposed site plan  
(existing garage shown  
dotted)



Site Location Plan



Tree Reference Plan

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